

**A. REPORT TO THE HEARING EXAMINER**

**HEARING DATE:** August 16, 2016

**Project Name:** Renton Commons

**Owners:** Low Income Housing Institute; 2407 First Ave; Seattle, WA 98121

**Contact:** Pamela Derry; Tonkin Architecture; 204 First Ave; Seattle, WA 98104

**File Number:** LUA16-000425, CU-H, SA-H, ECF, MOD, MOD, MOD

**Project Manager:** Rocale Timmons, Senior Planner

**Project Summary:** The Low Income Housing Institute (LIHI) is requesting Hearing Examiner Site Plan Review, Hearing Examiner Conditional Use Permit, Environmental 'SEPA' Review, and three (3) modifications for the construction of a 6-story building containing 48 affordable multi-family residential units. The structure would have an average height of 71 feet. The 0.32 acre site is located within the Center Downtown (CD) zoning classification on the west side of Whitworth Ave S, just north of S 3rd St, at 215 Whitworth Ave S. Vehicular access to the site would be provided via a single entry point from Whitworth Ave S. A total of 12 parking spaces would be provided within the structure. A refuse and recycle modification, from RMC 4-4-090, is being requested in order to reduce the number of required deposit and collection points. A bicycle parking modification, from RMC 4-4-080, is being requested in order to locate bicycle parking exterior to the structure. Finally, the applicant is requesting a street modification, from RMC 4-6-060, in order to maintain the existing 60-foot right-of-way, without dedication, and alter the required street cross section. The site is located within a Seismic Hazard Area. There appears to be no other critical areas located on site.

**Site Area:** 13,915 SF

**Total Building Area GSF:** 46,215 SF

**Project Location:** 215 Whitworth Ave S



Project Location Map

**B. EXHIBITS:**

- Exhibit 1-10: ERC Report Exhibits (dated July 25, 2016)
- Exhibit 11: Staff Recommendation to the Hearing Examiner (dated August 8, 2016)
- Exhibit 12: SEPA Determination and Mitigation Measures (dated July 25, 2016)
- Exhibit 13: City Center Community Plan
- Exhibit 14: Lighting Plan
- Exhibit 15: Downtown Streetscape Design Standards and Guidelines (DSDSG) (dated March 16, 2016)
- Exhibit 16: Renderings

**C. GENERAL INFORMATION:**

1. **Owner(s) of Record:** Low Income Housing Institute  
2407 First Ave  
Seattle, WA 98121
2. **Zoning Classification:** Center Downtown (CD)
3. **Comprehensive Plan Land Use Designation:** Commercial Mixed Use (CMU)
4. **Existing Site Use:** Single Family Residence
5. **Neighborhood Characteristics:**
- a. **North:** *Seattle Public Utility's Cedar River Pipeline ROW (CD Zone)*
  - b. **East:** *Church (CD Zone)*
  - c. **South:** *Church (CD Zone)*
  - d. **West:** *Single Family Residential (CD Zone)*
6. **Site Area:** 13,915 SF (.03194 acres)

**D. HISTORICAL/BACKGROUND:**

<u>Action</u>	<u>Land Use File No.</u>	<u>Ordinance No.</u>	<u>Date</u>
Comprehensive Plan	N/A	5758	06/22/2015
Zoning	N/A	5758	06/22/2015
Annexation	N/A	156	05/18/1909

**E. PUBLIC SERVICES:**

1. **Existing Utilities**
- a. Water: Water service is provided by the City of Renton. The site is in the 196' hydraulic pressure zone. The approximate static water pressure is 71 psi at a ground elevation of 32 feet. There is an existing 4-inch water main (COR Facility ID: WM-08542) east of the site along the eastern frontage of Whitworth Ave S. There are also two Seattle Public Utilities (SPU) transmission mains (66-inch and 51-inch) north of the site.
  - b. Sewer: Sewer service is provided by the City of Renton. There is an existing 6-inch sewer (COR Facility ID: GM06709) east of the site in Whitworth Ave S that flows south.
  - c. Surface/Storm Water: There is no existing on-site drainage system. There is an existing 6-inch storm drain east of the site in Whitworth Ave S, which flows north. There is also an existing 12-inch storm drain north of the site in the Seattle Public Utilities right of way, which flows west.
2. **Streets**: The site is bordered by Whitworth Ave S to the east. Whitworth Ave S is classified as a residential access street. The existing right of way width for this section of Whitworth Ave S is 60-feet. Sidewalk is located on both sides of the street at the back of curb. Sidewalk width along the project frontage is approximately 10-feet.
3. **Fire Protection**: City of Renton Fire Department

**F. APPLICABLE SECTIONS OF THE RENTON MUNICIPAL CODE:****1. Chapter 2 Land Use Districts**

- a. Section 4-2-020: Purpose and Intent of Zoning Districts
- b. Section 4-2-070: Zoning Use Table
- c. Section 4-2-120: Commercial Development Standards

**2. Chapter 3 Environmental Regulations**

- a. Section 4-3-100: Urban Design Regulations

**3. Chapter 4 Property Development Standards****4. Chapter 6 Streets and Utility Standards**

- a. Section 4-6-060: Street Standards

**5. Chapter 9 Permits – Specific**

- a. Section 4-9-030: Conditional Use Permits
- b. Section 4-9-200: Site Plan Review
- c. Section 4-9-250: Variances, Waivers, Modifications, and Alternates

**6. Chapter 11 Definitions****G. APPLICABLE SECTIONS OF THE COMPREHENSIVE PLAN:**

1. Land Use Element (LU)
2. Housing and Human Services Element (HHS)

**H. FINDINGS OF FACT (FOF):**

1. The applicant is requesting Hearing Examiner Site Plan Review, Hearing Examiner Conditional Use Permit, Environmental 'SEPA' Review, and three (3) modifications for the construction of a 6-story building containing 48 affordable multi-family residential units.
2. Renton Commons will include a set-aside of 36 units for homeless families and individuals, including veterans, who will have access to on-site supportive services, job connections and case managers to help people improve their incomes, get back on their feet, retain their self-sufficiency, strengthen their physical and mental health and progress in their lives.
3. The northeastern portion of the site is developed with a two-story single family residence, proposed for demolition. The undeveloped portion of the site is vegetated with grass, shrubbery and scattered trees.
4. The development would result in a density of 150.28 du/ac. The proposed 48 units would be comprised of (4) studio units, (19) 1-bedroom units, (20) 2-bedroom units, and (5) 3-bedroom units.
5. The project would incorporate sustainable “green” features meeting the requirements of the Washington State Commerce Department’s Evergreen Sustainable Design Standard.
6. The Planning Division of the City of Renton accepted the above master application for review on June 9, 2016 and determined the application complete on June 22, 2016. On June 30, 2016 a re-notice occurred, in order to correct the proposed project description. The project complies with the 120-day review period.

7. The project site is located on the west side of Whitworth Ave S, between S 2<sup>nd</sup> St and S 3<sup>rd</sup> St. The site is generally rectangular in shape and totals 13,915 square feet in area (0.3194 acres).
8. The site is located within the Center Downtown (CD) zoning classification, Commercial Mixed Use (CMU) Comprehensive Plan land use designation, Design District 'A', and the Downtown Business District.
9. Surrounding uses include: existing single family residential to the west (zoned CD); churches to the east and south (zoned CD); and an office building to the north for an international adoption agency (zoned CD). It should be noted that immediately north of the site is a Seattle Public Utility's Cedar River pipeline right-of-way and an 11-foot wide public alley is abutting the property to the west.
10. Access to the site is proposed via a new curb cut extended from Whitworth Ave S.
11. The proposed building includes an open surface parking garage on the west side of the property, which includes 12 parking spaces. An additional 3-parking stalls are proposed to be secured, via a lease with SPU, in the parking area abutting the site to the north.
12. Pursuant to the City of Renton's Environmental Ordinance and SEPA (RCW 43.21C, 1971 as amended), on July 25, 2016, the Environmental Review Committee issued a Determination of Non-Significance - Mitigated (DNS-M) for the Renton Commons project. The DNS-M included one mitigation measure (Exhibit 12). A 14-day appeal period commenced on July 29, 2016 and will end on August 12, 2016. No appeals of the threshold determination have been filed as the date of this report.
13. Based on an analysis of probable impacts from the proposal, the Environmental Review Committee (ERC) issued the following mitigation measure with the Determination of Non-Significance – Mitigated:
  - a. A survey shall be submitted to the Current Planning Project Manager that conforms to the requirements and standards of the Washington State Office of Archaeology and Historic Preservation and must be conducted under the on-site supervision of a state-approved archaeologist prior to construction permit approval. In addition to the survey, should evidence of a historic site be found during site development, work shall immediately cease and the Washington State of Archaeology and Historic Preservation shall be contacted at (360) 586-3065. In the event that cultural artifacts are found, work cannot recommence until approval is received from the Office of Archaeology and Historic Preservation.
14. The proposed building massing would be L-shaped with an outdoor play area. The building would also provide amenity spaces for its residents such as a multi-purpose common room, a library, community computer stations, a community kitchen for events, a classroom, a management office, and a counseling office.
15. The tallest point of the structure would be approximately 76-feet from the average grade plane to the highest peak of a shed roof elements with an average height of 71-feet. The proposed building materials would be a combination of concrete masonry, brick, metal canopy, cast-inplace concrete, fiber cement board, and wood elements. All concrete walls are proposed to be treated with texturing and/or reveals (Exhibit 4).
16. **Requested Modifications from RMC:** The following modifications have been requested to be considered simultaneously as part of the Site Plan Review:

RMC Code Citation	Required Standard	Requested Modification
RMC 4-6-060F Street Standards	12-foot sidewalk, street trees (4-foot x 8-foot grates) behind the curb, and street lighting meeting City's arterial	Elimination of the 5-foot wide bicycle lane.

	street lighting levels. Additionally, a 5-foot wide bike lane is required.	
RMC 4-4-080F, Parking, Loading, and Driveway Regulations	Based on the proposed use, a minimum 24 interior bicycle parking spaces would be required.	The applicant is proposing a total of 24 bicycle parking spaces within four exterior bike racks.
RMC 4-4-090, Refuse and Recyclables Standards	There shall be at least one deposit area/collection point for every thirty dwelling units.	The proposal includes a single refuse/recycle storage area.

17. The site is located in Seismic Hazard Area. There are no other known critical areas on site.
18. There are three existing trees located within the right-of-way which are proposed for removal. Preliminary earthwork for the proposal includes 100 cubic yards of excavation and 300 yards of fill.
19. Construction is expected to start in mid-September 2017. Construction is expected to be completed by the end of October 2018.
20. Studies provided by the applicant include a stormwater report, traffic study, arborist report, preliminary archeology assessment, and a geotechnical report (Exhibit 6-8, and 10).
21. Staff received a single comment letter, from the Department of Archaeology and Historic Preservation, regarding the potential of archaeological deposits on site (Exhibit 9). No other public comments were received.
22. On page two of the staff report, to the Environmental Review Committee, it states the applicant is proposing a 19,795 square foot landscape community space and goes on to describe the space (Exhibit 1). This is an error, and necessitates a correction for the record. The proposed exterior open space (playground and courtyard) is a total of 1,458 square feet. There is an additional 1,169 square feet of community space on the interior of the structure.
23. Representatives from various city departments have reviewed the application materials to identify and address issues raised by the proposed development. These comments are contained in the official file, and the essence of the comments has been incorporated into the appropriate sections of this report and the Departmental Recommendation at the end of this report.
24. **Comprehensive Plan Compliance:** The site is designated Commercial Mixed Use (CMU) on the City's Comprehensive Plan Map. The intention of this designation is to transform strip commercial development into business districts through the intensification of uses and with cohesive site planning, landscaping, signage, circulation, parking, and the provision of public amenity features. The proposal is compliant with the following Comprehensive Plan Goals and Policies if all conditions of approval are met:

Compliance	Comprehensive Plan Analysis
✓	<b>Policy L-2:</b> Support compact urban development to improve health outcomes, support transit use, maximize land use efficiency, and maximize public investment in infrastructure and services.
✓	<b>Goal L-H:</b> Plan for high-quality residential growth that supports transit by providing urban densities, promotes efficient land utilization, promotes good health and physical activity, builds social connections, and creates stable neighborhoods by incorporating both built amenities and natural features.
✓	<b>Goal L-BB:</b> Maintain a high quality of life as Renton grows by ensuring that new development is designed to be functional and attractive.

✓	<b>Goal L-FF:</b> Strengthen the visual identity of Renton and its Community Planning Areas and neighborhoods through quality design and development.
	<b>Policy L-46:</b> Identify and catalog historic, cultural, and archaeological resources on an on-going basis and as part of project specific review.
✓	<b>Policy L-51:</b> Respond to specific site conditions such as topography, natural features, and solar access to encourage energy savings and recognize the unique features of the site through the design of subdivisions and new buildings.
✓	<b>Policy L-52:</b> Include human-scale features such as pedestrian pathways, quality landscaping, and public spaces that have discernible edges, entries, and borders to create a distinctive sense of place in neighborhoods, commercial areas, and centers.
✓	<b>Policy L-53:</b> Orient buildings in developments toward the street or a common area, rather than toward parking lots.
✓	<b>Policy L-57:</b> Complement the built environment with landscaping using native, naturalized, and ornamental plantings that are appropriate for the situation and circumstance and which provide for respite, recreation, and sun/shade.
✓	<b>Policy L-60:</b> Thoughtfully balance the need for appropriate lighting levels for safety and security to avoid light intrusion and glare impacts, and to preserve the night sky.
✓	<b>Policy L-64:</b> Collaborate with schools, businesses, and faith-based groups to promote healthy lifestyles through education, activity and nutrition
✓	<b>Policy HHS-3:</b> Work with other jurisdictions and organizations, including the Renton Housing Authority and non-profit housing developers, to address the need for housing to be affordable to very low-income households. This housing should focus on accessibility, mobility, and proximity to social services.
✓	<b>Policy HHS-14:</b> Work with other jurisdictions and social service organizations to develop a regional approach to end homelessness and, on a local level, with community partners to expand services available to the chronically homeless, temporarily homeless, and other vulnerable populations in Renton.
✓	<b>Policy HHS-25:</b> Encourage construction of universally designed units, supportive housing arrangements, and transitional housing in close proximity (within one-quarter mile) to public transportation.

**25. Zoning Development Standard Compliance:** The CD Zone is intended to revitalize the area by creating a vibrant, urban center in Renton's historic downtown core. Surface parking is discouraged in this zone, except as a land bank. The proposal is compliant with the following development standards if all conditions of approval are met:

Compliance	CD Zone Develop Standards and Analysis
<b>See FOF 28: Conditional Use Permit Analysis</b>	<b>Density:</b> Per RMC 4-2-120B the allowed density range in the CD zoning classification is a minimum of 25 dwelling units per net acre (du/ac) up to a maximum of 100 du/ac. Density may be increased to 150 dwelling units per net acre subject to Administrative Conditional Use approval. Net density is calculated after public rights-of-way, private access easements, and critical areas are deducted from the gross acreage of the site. Fractions which result from net density calculations shall be truncated at two (2) numbers past the decimal (e.g., 4.5678 becomes 4.56). Calculations for minimum or maximum density which result in a fraction that is 0.50 or greater shall be rounded up

	<p>to the nearest whole number. Those density calculations resulting in a fraction that is less than 0.50 shall be rounded down to the nearest whole number.</p> <p><i>Staff Comment:</i> There would be no deductions from the 13,915 gross square foot (0.3194 acres) site for critical areas, right-of-way dedications, or access easements. The 48 unit proposal would arrive at a net density of 150 dwelling units per acre (48 units / 0.3194 acres = 150.28 du/ac rounded down to the nearest whole number). Therefore the proposal requires an Administrative Conditional Use Permit.</p> <p>Pursuant to RMC 4-8-080C.2 where required permits are subject to different types of permit review procedures, then all the applications are subject to the highest-number procedure. Pursuant to RMC 4-9-200D.2 a public hearing before the Hearing Examiner is required for Site Plan Review when the structure is taller than four stories. Therefore, the requested Conditional Use Permit is also required to be reviewed by the Hearing Examiner.</p> <p>See Conditional Use Permit analysis under Finding 28: Conditional Use Permit Analysis.</p>								
✓	<p><b>Lot Dimensions:</b> Per RMC 4-2-120B there is no minimum lot size, width or depth in the CD zone.</p> <p><i>Staff Comment:</i> The proposal does not propose to alter any lot lines.</p>								
✓	<p><b>Lot Coverage:</b> Per RMC 4-2-120B there is no maximum lot coverage for buildings within the CD zone.</p> <p><i>Staff Comment:</i> The proposed building would have a footprint of 8,294 square feet on the 13,915 square foot site resulting in a building lot coverage of approximately 60 percent.</p>								
Compliant if Condition of Approval is Met	<p><b>Setbacks:</b> Per RMC 4-2-120B the CD zoning classification has a minimum front yard and side yard along-a-street setback of 0-feet and a maximum setback of 15 feet for portions of the structure that are below 25 feet in height. There is no maximum front yard, or side yard along-a-street setback for those portions of the structure that exceed 25 feet in height. There are no other setback requirements within this zone.</p> <p><i>Staff Comment:</i> The following table contains setbacks, at the closest point, for the proposed structure:</p>								
	<table><tr><th>West Setback (Alley)</th><th>East Setback (Whitworth Ave)</th><th>North Setback (SPU ROW)</th><th>South Setback</th></tr><tr><td>6-feet and 7-inches</td><td>2-feet and 3-inches</td><td>2-feet</td><td>10-feet and 3 inches</td></tr></table>	West Setback (Alley)	East Setback (Whitworth Ave)	North Setback (SPU ROW)	South Setback	6-feet and 7-inches	2-feet and 3-inches	2-feet	10-feet and 3 inches
	West Setback (Alley)	East Setback (Whitworth Ave)	North Setback (SPU ROW)	South Setback					
6-feet and 7-inches	2-feet and 3-inches	2-feet	10-feet and 3 inches						
<p>The proposed structure complies with all setbacks of the CD zone. It should be noted that in order to provide more varied and interesting modulation on the upper façade facing the street—Whitworth Ave S, and to make the main building entrance more prominent, the applicant is proposing to overhang the right-of-way by a maximum of two feet at the entrance canopy and at the central bay on the building’s east and main façade. The central bay is proposed to begin at the third floor at a height of approximately 25 feet above grade. The distance above grade would ensure that the encroachment over the sidewalk portion of the right-of-way would have no adverse impact on pedestrians on the sidewalk. The proposed bay window would require the applicant to obtain a right-of-way use permit.</p> <p>Therefore staff recommends a condition of approval, requiring the applicant to obtain a permanent right-of-way use permit in order to encroach into right-of-way by no</p>									

	<i>more than two –feet for the central bay. The right-of-way use permit shall be obtained prior to construction/building permit approval.</i>
<b>Compliant if Condition of Approval is Met</b>	<p><b>Building Height:</b> Per RMC 4-2-120B building height is restricted to 95 feet.</p> <p><u>Staff Comment:</u> The applicant has provided a varied roofline consisting of a continuous roof cornice which would wrap the top of the facades, providing a strong horizontal line that is interrupted by taller bays with pitched shed roofs for visual interest (Exhibit 4 and 16). The height of the proposed structure would be 76-feet at the tallest point of the shed roof elements with an average height of 71-feet. The proposal complies with the height requirement of the zone. The Airport overlay, Part 77 horizontal surface height limit is 179 feet above mean sea level in this area. Staff recommends a condition of approval requiring the applicant to demonstrate compliance with the Part 77 Horizontal Surface Height Limit prior to construction/building permit approval.</p>
<b>Compliant if Conditions of Approval is Met</b>	<p><b>Landscaping:</b> Per RMC 4-4-070 all development in the CD zone is exempt from all but the maintenance of any existing landscaping, landscaping for surface parking areas, and street tree requirements of this Section.</p> <p><u>Staff Comment:</u> While there are no on-site landscape requirements in the CD zone, with the exception of surface parking areas and street trees, a conceptual landscape plan was submitted with the project application (Exhibit 3). The conceptual landscape plan illustrates materials that would be used to enhance the visual character of the building.</p> <p>The proposed street level landscaping utilizes street trees in 5-foot x 5-foot tree grates used to highlight architectural features and create visual interest along the streetscape. The proposed street trees are Rocky Mountain glow maple.</p> <p>Along the north façade, the applicant is proposing a mix of vegetation in order to soften the facades of the utility spaces and also help define the new sidewalk along the SPU right-of-way. At the northwest corner, the applicant is proposing a gazebo which would mark the terminus of the pedestrian experience until the property to the west is developed and possibly extends the new walkway through the block to Shattuck Ave S.</p> <p>The landscaping along the west façade and the southwest corner would be a combination of soft, colorful ground cover and small ornamental trees that would serve to soften the concrete walls and help screen the parking garage.</p> <p>At the southeast corner, the main outdoor area for residents and visitors, trellises, seating, covered bike racks, and play equipment are proposed.</p> <p>The landscape plan includes a planting plan which contains different tree (vine maple, Kousa dogwood, galaxy magnolia, Japanese maple, shore pine, and Autumn brilliance serviceberry) and shrub species (smoketree, spike winter hazel, common lilac, heavenly bamboo, snowberry, deer fern, yellowtwig dogwood, goldflame spiraea). It should also be noted the applicant is proposing green roof elements.</p> <p>The proposed landscape plan does not provide specific detail for proposed screening, landscape elements, and furniture. The City has begun to draft the Downtown Streetscape Design Standards and Guidelines (DSDSG) of which the most recent standards are dated March 16, 2016 (Exhibit 15). The DSDSG include specifications for street and public realm elements, treatments and surfaces (benches, tree grates, bike racks, street lights, litter receptacles, etc.). Compliance with the standard elements,</p>

	<p>included in this document, will serve to comply with the landscaping requirements, as well as many of the design standards (see additional discussion under FOF 27: Design District Review), and would allow for a more consistent urban design signature that reinforces the Downtown identity. Therefore staff recommends, as a condition of approval, the applicant be required to submit a revised landscape plan to the Current Planning Project Manager prior to building permit approval. The revised landscape plan shall include the following: compliance with the draft Downtown Streetscape Design Standards and Guidelines (DSDSG) (dated March 16, 2016) or as may be amended or approved prior to plans being submitted for building permits; specific detail for courtyard screening and furniture; the addition of one litter receptacle to the frontage improvements located as close to primary building entrance as possible; and landscape planters to denote pedestrian entry points.</p> <p>It should be noted existing landscaping between the subject property and the three proposed joint parking spaces, located within the SPU right-of-way, has died. The applicant is encouraged to replace the landscaping as part of the proposed project.</p>								
✓	<p><b>Tree Retention:</b> The City’s adopted Tree Retention and Land Clearing Regulations require the retention of 10 percent of trees in a commercial development.</p> <p>Significant trees shall be retained in the following priority order:</p> <p>Priority One: Landmark trees; significant trees that form a continuous canopy; significant trees on slopes greater than twenty percent (20%); Significant trees adjacent to critical areas and their associated buffers; and Significant trees over sixty feet (60') in height or greater than eighteen inches ( 18") caliper.</p> <p>Priority Two: Healthy tree groupings whose associated undergrowth can be preserved; other significant native evergreen or deciduous trees; and Other significant non- native trees.</p> <p>Priority Three: Alders and cottonwoods shall be retained when all other trees have been evaluated for retention and are not able to be retained, unless the alders and/ or cottonwoods are used as part of an approved enhancement project within a critical area or its buffer.</p> <p><u>Staff Comment:</u> The site does not contain any significant trees on site and is therefore exempt from tree retention requirements found in RMC 4-4-130. The applicant however is proposing to plant 21 new trees on site (Exhibit 3).</p>								
Compliant if Conditions of Approval are Met	<p><b>Parking:</b> The parking regulations, RMC 4-4-080, require a specific number of off-street parking stalls be provided based on the square footage of the use. Pursuant to RMC 4-2-120B all parking shall be provided in the rear portion of the yard, with access taken from an alley, when available. Parking shall not be located in the front yard, nor in a side yard facing the street nor rear yard facing the street. Parking may be located off-site or subject to a joint parking requirement.</p> <p><u>Staff Comment:</u> The following ratios would be applicable to the site:</p> <table><tr><th>Use</th><th># of residential units</th><th>Ratio</th><th>Required Spaces</th></tr><tr><td>Affordable Residential Units</td><td>48</td><td>Min: 0.25 space / residential unit  Max: 1 space / residential unit</td><td>Min: 12  Max: 48</td></tr></table>	Use	# of residential units	Ratio	Required Spaces	Affordable Residential Units	48	Min: 0.25 space / residential unit  Max: 1 space / residential unit	Min: 12  Max: 48
Use	# of residential units	Ratio	Required Spaces						
Affordable Residential Units	48	Min: 0.25 space / residential unit  Max: 1 space / residential unit	Min: 12  Max: 48						

	<p><i>Based on the proposed uses, a minimum of 12 parking spaces would be required in order to meet code up to a maximum of 48 stalls. The applicant proposed a total of 12 spaces within structured parking (4 standard stalls, 6 compact stalls, and 2 ADA stalls). Additionally, the applicant is proposing to lease three parking stalls, on the SPU right-of-way abutting the project site to the north, for staff parking.</i></p> <p><i>Staff would like to note while the proposal complies with the parking ratios of the code and dimensional standards, the proposed project is anticipated to not only create new jobs but create 36 housing units which are not supported by parking. Since the proposed housing is for extremely low income families and individuals (half 30% and half 50% AMI), many of the tenants are not expected to own cars and LIHI's experience on other projects in the area suggests that 12 parking spaces would be more than sufficient for the needs of these tenants. The project site is also in very close proximity to the Renton Transit Center and a wide range of shopping options are within easy walking distance of the site which is ideal for tenants without vehicles.</i></p> <p><i>The applicant is proposing, as part of their leasing process, a declaration that on-site parking is very limited. It is the applicant's intention to create a process which gives priority for parking spaces to (1) the disabled; (2) families with children who are car owners; and (3) commuters for whom taking public transit is not feasible. It is also the intent of applicant to allow no more than one car per unit to be garaged, if they are within the priority populations noted above.</i></p> <p><i>Given three of the proposed parking stalls are located off site, staff is recommending a condition of the approval requiring the applicant provide a copy of a joint parking agreement for those parking stalls located off site. The joint parking agreement shall be submitted to the Current Planning Project Manager prior to building permit approval.</i></p> <p><i>Additionally, the proposal does not appear to include van accessible parking. Therefore, staff recommends the provision of an ADA van accessible parking stall. The revised parking plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</i></p>
<p><b>See FOF 31: Refuse and Recycle Modification Analysis</b></p>	<p><b>Refuse and Recyclables:</b> Per RMC 4-4-090 for multi-family developments a minimum of 1 ½ square feet per dwelling unit is required for recyclable deposit areas and a minimum of 3 square feet per dwelling unit is required for refuse deposit areas. There shall be at least one deposit area/collection point for every thirty (30) dwelling units.</p> <p><i><u>Staff Comment:</u> Based on the proposal for a total 48 residential units, 216 square feet of refuse and recycle area is required to be dedicated. The proposal includes a 373 square foot area dedicated to refuse and recycle which complies with the area dedication requirements. The applicant is proposing only one collection point for the building as opposed to the two that would be required pursuant to code.</i></p> <p><i>Pursuant to RMC 4-9-250D the applicant is requesting an Administrative Modification from RMC 4-4-090, Refuse and Recyclable Standards in order to reduce the number of collections points for the refuse and recycle area.</i></p> <p><i>Staff is in support of the requested modification if all conditions of approval are met. See additional discussion below in FOF 31: Refuse and Recycle Modification Analysis.</i></p>

**26. Site Plan Review:** Pursuant to RMC 4-9-200.B, Site Plan Review is required for development in the CD zoning classification when it is not exempt from Environmental (SEPA) Review. Site Plan applications

are evaluated for compliance with the specific requirements of the RMC 4-9-200.E.3 the following table contains project elements intended to comply with level of detail needed for the Site Plan review requests:

Compliance	Site Plan Criteria and Analysis
<i>Compliant if Conditions of Approval are Met</i>	<p><b>a. Comprehensive Plan Compliance and consistency.</b></p> <p><u>Staff Comment:</u> See previous discussion under FOF 24: Comprehensive Plan Analysis.</p>
<i>Compliant if Conditions of Approval are Met</i>	<p><b>b. Zoning Compliance and Consistency.</b></p> <p><u>Staff Comment:</u> See discussion under FOF 25: Zoning Development Standard Compliance.</p>
<i>Compliant if Conditions of Approval are Met</i>	<p><b>c. Design Regulation Compliance and Consistency.</b></p> <p><u>Staff Comment:</u> See discussion under FOF 27: Design District Review.</p>
N/A	<b>d. Planned action ordinance and Development agreement Compliance and Consistency.</b>
<i>Compliant if Conditions of Approval are Met</i>	<p><b>e. Off Site Impacts.</b></p> <p><b>Structures:</b> Restricting overscale structures and overconcentration of development on a particular portion of the site.</p> <p><u>Staff Comment:</u> While the proposed mixed-use structure is concentrated over most of the site. The building's façade would be articulated and modulated in order to divide larger architectural elements into small increments. Large horizontal material changes along the bottom floor and the upper two floors would help to reduce the perceived height of the building mass. Smaller-scaled design elements such as the windows, cornices, canopies, plus the textures and colors of the façade materials would also help provide a well-balanced and well-proportioned building.</p> <p>The applicant is proposing a few bay windows on each façade that would extend up past the top of the roof parapet. These tall bay windows would be capped by pitched shed roofs to provide additional articulation and interest, especially when viewed from afar. A continuous roof cornice is proposed to wrap the top of the facades, providing a strong horizontal line that is interrupted only by the taller bays with the pitched shed roofs.</p> <p>The building mass would be further modulated by a large vertical notch at the inside corner of the south façade. To help balance the strong vertical elements of the bay windows and the recessed notch, the applicant has proposed exterior cladding materials which serve to create strong horizontal lines along the ground floor and the top two floors.</p> <p>At the ground floor the common spaces would be clad in stained horizontal cedar slats and the parking/utility spaces would be clad in concrete with strong horizontal reveals aligned with the wall openings.</p> <p>At floors two through four the facades would be clad in cementitious panel siding with a composition of reveals providing subtle horizontal lines at the tops and bottoms of the window openings. The cladding on the upper two floors, 5 and 6 would be a</p>

combination of horizontal cementitious panel and lapsiding.

*Varying paint colors are intended to further emphasize the various parts of the exterior composition; the distinct bay windows, building masses, base/middle/top of the façades and also smaller scale accent panels that provide additional interest and help tie the various exterior elements together into a single expression that is expected to be attractive from both near and far.*

*See additional discussion under FOF 27, Design District Review.*

**Circulation:** Providing desirable transitions and linkages between uses, streets, walkways and adjacent properties.

Staff Comment: *The applicant is proposing to access the structured parking garage via a single curb cut from Whitworth Ave S. Off-site street improvements are proposed along Whitworth Ave S which includes a new 12-foot wide sidewalk providing linkages to the existing sidewalk network.*

*In order to build a 12-foot wide sidewalk, without modifying the alignment of the right-of-way curb requires the sidewalk to encroach on to the subject site by approximately two feet. The applicant is proposing to build the sidewalk with this extension onto the property, without a right of way dedication. This would allow the full sidewalk width without modifying the curb alignment, but would also allow the upper floors of the building to overhang the sidewalk extension into the parcel. A pedestrian easement would insure that this sidewalk extension remains available to the public. Therefore, staff recommends as a condition of approval, the applicant be required to submit a recorded pedestrian easement in sufficient width (up to 2 feet) to construct required street improvements behind the existing curb. The easement shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval.*

*Finally, the proposed plan does not include a suitable transition to the adjacent sidewalk north and south of the site. The transition of new sidewalk may need to be extended slightly beyond property frontage to the north and south sides and it would likely need to be field verified where the transition should occur. Therefore staff recommends a condition of approval requiring the applicant to submit a revised paving plan which includes a suitable transition to the existing sidewalk, north and south of the subject property. The revised paving plan shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval.*

**Loading and Storage Areas:** Locating, designing and screening storage areas, utilities, rooftop equipment, loading areas, and refuse and recyclables to minimize views from surrounding properties.

Staff Comment: *The proposed refuse and recycle area is located within the structure along the northwestern portion (rear) of the site to avoid having the trash room highly visible from the public.*

*The bins are proposed to have wheels and with the assistance of a trash caddy the on-site manager would be able to bring the bins to the curb for pick up and return the bins to the trash room after pick up. An exterior staging area for trash and recycling is proposed to be provided directly exterior to the trash/recycling room. Care will be needed to ensure that the bins do not leak once they exit the building and moved toward the pick-up area.*

*With respect to temporary storage the description does not indicate if an on-site*

	<p><i>staging site is necessary given the limited space of the subject site. Therefore, staff recommends a condition of approval requiring the applicant to identify an off-site staging area for construction. The area shall be within a reasonable distance from site to limit construction traffic to and from the site. Pedestrian paths to and from Renton High School should be carefully studied to ensure a well-coordinated, signed, and maintained traffic control plan. The traffic control plan shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval. Additionally, the applicant shall be required to notify surrounding property owners (within 300-foot radius of the site) in advance of the start of construction (no less than 30 days), and provide updates no less than quarterly during the construction period. This would ensure students walking to Renton High School and other businesses in the vicinity of the subject site have safe walking conditions.</i></p> <p><b>Views:</b> Recognizing the public benefit and desirability of maintaining visual accessibility to attractive natural features.</p> <p><u>Staff Comment:</u> <i>There are no territorial views for which to maintain visual accessibility with the exception of potential views. Staff received no comments from adjacent properties regarding views.</i></p> <p><i>The proposed mixed use structure would establish new territorial views to the north as well as potential visual access to Mt. Rainier to the south.</i></p> <p><b>Landscaping:</b> Using landscaping to provide transitions between development and surrounding properties to reduce noise and glare, maintain privacy, and generally enhance the appearance of the project.</p> <p><u>Staff Comment:</u> <i>See discussion under FOF 25: Zoning Development Standard Compliance, Landscaping.</i></p> <p><b>Lighting:</b> Designing and/or placing exterior lighting and glazing in order to avoid excessive brightness or glare to adjacent properties and streets.</p> <p><u>Staff Comment:</u> <i>A lighting plan was provided with the application (Exhibit 14).It is anticipated that exterior lighting fixtures are located to provide light on the outdoor areas around the parcel for safety and security, but would not spill onto neighboring properties.</i></p> <p><i>However, concerns have been raised regarding the adequacy of lighting along the western property line, the adjacent alley, and structured parking area. See additional discussion under FOF 26: Site Plan Review Analysis, Internal Circulation.</i></p> <p><i>Finally street lights along the Whitworth Ave S frontage would be required to be replaced by pedestrian level light pole(s). It is unclear the number of poles and distance separation needed for a demonstration of compliance with required illumination levels pursuant to RMC 4-6-060.I. Therefore, staff recommends the applicant be required to submit a lighting analysis demonstrating compliance with RMC 4-6-060.I.3. The lighting analysis shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval.</i></p>
<p><b>Compliant if Condition of Approval is Met</b></p>	<p><b>f. On Site Impacts.</b></p> <p><b>Structure Placement:</b> Provisions for privacy and noise reduction by building placement, spacing and orientation.</p> <p><u>Staff Comment:</u> <i>The exterior common spaces are located adjacent to the public right-of-ways and the existing parking lot to the south not close to adjacent buildings. There</i></p>

are also no occupied exterior terraces or roof decks above the ground level that could project noise over the adjacent buildings to the larger area. All noise from exterior common spaces at ground level would be limited and contained by hardscape and landscape features such as trees, canopies, trellises, and fencing.

**Structure Scale:** Consideration of the scale of proposed structures in relation to natural characteristics, views and vistas, site amenities, sunlight, prevailing winds, and pedestrian and vehicle needs.

Staff Comment: The proposed building massing would be L-shaped with an outdoor play area and a vehicle access driveway located on the southeast corner of the site. This design will provide the maximum amount of natural sunlight to both the outdoor common spaces and the residential units above.

The pedestrian courtyard has been strategically placed on site in order take advantage of sun exposure from the south most times of the year and would likely only be shaded at certain times of the day during the winter months.

Revisions to the entrance canopy, exterior lighting elements, and the provision of streetscape elements along Whitworth Ave S are recommended to be used to reinforce the pedestrian scale and orientation of the ground floor frontage (see FOF 27: Design District Analysis). If all recommended conditions of approval are met the building's ground floor street frontage would be visually distinct from the upper floors of the building to create a well-defined, pedestrian-scaled base.

**Natural Features:** Protection of the natural landscape by retaining existing vegetation and soils, using topography to reduce undue cutting and filling, and limiting impervious surfaces.

Staff Comment: The site does not contain any significant trees on site and therefore exempt from tree retention requirements found in RMC 4-4-130.

The site can best be characterized as relatively flat with the western side of the site sloping gently down toward the southwest to northwest from a high point near the center of the site. The northwest corner of the site has the lowest elevation. There is a change in elevation on the order of five feet across the site. The majority of the slopes on site are moderate, ranging from 1-4%. The steepest slope on the site is approximately 32%, which is a man-made depression which is proposed to be leveled as part of construction.

The applicant is proposing excavation in the amount of approximately 100 cubic yards. Approximately 300 cubic yards of fill is proposed. Following construction the applicant is proposing an impervious cover of approximately 88% of the net site area. It should be noted that a 1,164 square foot vegetated roof is proposed.

The applicant submitted a Geotechnical Report prepared by Geotech Consultants, dated July 21, 2015 (Exhibit 6). The loose to medium-dense sand soils that are saturated are susceptible to seismic liquefaction. As a result, the proposal includes a deep foundation system supported by pipe piles to reduce the potential for uneven settlement. Groundwater seepage was observed at depths of 12-14 feet. The seepage levels represent transient water seepage water and likely do not indicate static groundwater.

The geotechnical report includes specific recommendations in order to mitigate potential geotechnical impacts including: site preparation, structural fill, foundations, drainage considerations, hazards including, and project design and monitoring. The applicant will be required to demonstrate compliance with the recommendations

<b>Compliant if Conditions of Approval are Met</b>	<p><i>included in the provided Geotechnical Engineering Report with the building permit application and during construction (Exhibit 6).</i></p> <p><i>Removal of the existing vegetated cover during construction would leave soils susceptible to erosion. The applicant will be required to design a Temporary Erosion and Sedimentation Control Plan (TESCP) pursuant to the 2009 King County Surface Water Design Manual Erosion and Sediment Control Requirements.</i></p> <p><b>Landscaping:</b> Use of landscaping to soften the appearance of parking areas, to provide shade and privacy where needed, to define and enhance open spaces, and generally to enhance the appearance of the project. Landscaping also includes the design and protection of planting areas so that they are less susceptible to damage from vehicles or pedestrian movements.</p> <p><u>Staff Comment:</u> See FOF 24: Zoning Development Standard Compliance, Landscaping.</p>
	<b>g. Access</b>
	<p><b>Location and Consolidation:</b> Providing access points on side streets or frontage streets rather than directly onto arterial streets and consolidation of ingress and egress points on the site and, when feasible, with adjacent properties.</p> <p><u>Staff Comment:</u> Access to the site is proposed via a single entry to the structured parking via Whitworth Ave S. The proposal promotes safe and efficient circulation through a single access point.</p>
	<p><b>Internal Circulation:</b> Promoting safety and efficiency of the internal circulation system, including the location, design and dimensions of vehicular and pedestrian access points, drives, parking, turnarounds, walkways, bikeways, and emergency access ways.</p> <p><u>Staff Comment:</u> According to the Police Department, due to the heavy foot and vehicle traffic in the area of the proposed building, it is expected that the covered garage will be an attractive target to auto thieves and prowlers. Additionally, the alley west of the site has historically had safety concerns.</p> <p>Therefore staff recommends the applicant provide a lighting/safety plan which includes the following: lighting within the covered garage during all hours; lighting to illuminate the alley; a limited access gate for the garage; a surveillance system installed in the covered garage to help deter incidents of crime and suspicious activity; and an emergency communications device within the covered garage. The lighting/safety plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</p> <p>See Location and Consolidation discussion above and Pedestrian discussion below.</p>
	<p><b>Loading and Delivery:</b> Separating loading and delivery areas from parking and pedestrian areas.</p> <p><u>Staff Comment:</u> See FOF 26, Site Plan Review Analysis, Loading and Storage Areas.</p>
	<p><b>Transit and Bicycles:</b> Providing transit, carpools and bicycle facilities and access.</p> <p><u>Staff Comment:</u> Per RMC 4-4-080F.11.a 0.5 bicycle parking space is required per one dwelling unit. The proposal contains 48 units therefore, a minimum of 24 bicycle parking stalls shall be required for the residential units.</p> <p>Whenever possible, bicycle parking shall be incorporated into the building design. The applicant is proposing to provide four exterior bicycle racks with capacity for 24 bikes</p>

	<p><i>outside of the structure. Two of these racks with capacity for 16 bikes will be protected from weather by a canopy. Two short term bicycle racks at the public sidewalk with space for up to 4 bicycles will also be provided.</i></p> <p><i>Pursuant to RMC 4-9-250D the applicant is requesting an Administrative Modification from RMC 4-4-080, in order to locate bicycle parking outside of the structure. Staff is in support of the requested modification if all conditions of approval are met. See discussion below in FOF 29: Bicycle Parking Modification Analysis.</i></p> <p><b>Pedestrians:</b> Providing safe and attractive pedestrian connections between parking areas, buildings, public sidewalks and adjacent properties.</p> <p><u>Staff Comment:</u> <i>With the provision of a 2-foot wide pedestrian easement along Whitworth Ave S the existing 10-foot wide sidewalk would be widened to 12-feet. The increase in width would serve to reinforce the pedestrian network linkages and promote pedestrian activity. See FOF 26: Site Plan Review Analysis, Circulation.</i></p> <p><i>The applicant's intent for pavement design for the replaced and new sidewalk areas, is unclear. In order to ensure consistent pavement design with existing Downtown public sidewalk pavement design staff recommends the applicant provide a pavement design for Whitworth Ave S. The pavement design shall be submitted to, and approved by, the Current Planning Project Manager prior to construction permit approval.</i></p>
<b>Compliant if Condition of Approval is Met</b>	<p><b>h. Open Space:</b> Incorporating open spaces to serve as distinctive project focal points and to provide adequate areas for passive and active recreation by the occupants/users of the site.</p> <p><u>Staff Comment:</u> <i>Exterior open space will include a small children's play area at the southeast corner of the property and a quieter exterior courtyard with a gazebo, appropriate for older tenants, at the northwest corner. Interior common spaces will include a multipurpose community room with computer access for tenants and a kitchenette for community functions a TV/library room and a classroom with direct access to the building exterior for potential community events such as a homework club for school age children. On site counseling services will also be provided for the building's tenants</i></p> <p><i>The pergola/gazebo structure is located in the rear of property which could be problematic as proposed in a "dead" area with no adjacent ground floor windows and may become an attraction for loitering. Therefore, staff recommends a condition of approval requiring the applicant relocate the proposed pergola/gazebo closer to Whitworth Ave S for more natural visibility, possibly in front of classroom window/exit corridor to the north or an alternative location. A revised landscaping plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</i></p>
✓	<p><b>i. Views and Public Access:</b> When possible, providing view corridors to shorelines and Mt. Rainier, and incorporating public access to shorelines</p> <p><u>Staff Comment:</u> <i>The proposed structure would not block view corridors to shorelines or Mt. Rainier. The public access requirement is not applicable to the proposal.</i></p>
✓	<p><b>j. Natural Systems:</b> Arranging project elements to protect existing natural systems where applicable.</p> <p><u>Staff Comment:</u> <i>There are no natural systems located on site with the exception of</i></p>

	<i>drainage flows. See Drainage discussion below.</i>
	<b>k. Services and Infrastructure:</b> Making available public services and facilities to accommodate the proposed use:
	<p><b>Police and Fire.</b></p> <p><i>Staff Comment:</i> Police and Fire Prevention staff indicated that sufficient resources exist to furnish services to the proposed development; if the applicant provides Code required improvements and fees. A Fire Impact Fee would be applicable to the proposal, the current fees are based on a rate of \$495.10 per new multi-family unit.</p>
	<p><b>Water and Sewer.</b></p> <p><i>Staff Comment:</i> The site is served by the City of Renton for all utilities. It has been determined that the preliminary fire flow demand for the proposed development is 2,500 gpm which is greater than that the available maximum fire flow capacity. In order to provide water service for domestic and fire protection to the proposed project, the following water main improvements are required: installation of a 12-inch water main in Whitworth S from the existing 12-inch water main in S 3rd St to the existing 12-inch water main at the intersection of Whitworth Ave S and S 2nd S.</p> <p>There is an existing 6-inch sewer (COR Facility ID: GM06709) east of the site in Whitworth Ave S that flows south. The replacement of the existing 6-inch sewer main is not required as part of this project. This main will be replaced in the near future as part of a City of Renton capital improvement project.</p>
✓	<p><b>Drainage.</b></p> <p><i>Staff Comment:</i> The site is located within the Black River drainage basin. The existing roof drainage is collected via roof downspouts and conveyed to the existing PSS in Whitworth Ave S. Runoff from landscaped areas appears to sheet flow off the site. There is no formal site drainage collection or conveyance system evident.</p> <p>There is an existing 6-inch storm drain east of the site in Whitworth Ave S which flows north. There is also an existing 12-inch storm drain north of the site in the Seattle Public Utilities right of way which flows west.</p> <p>This project is required to comply with the 2009 King County Surface Water Manual and the City of Renton Amendments to the KCSWM, Chapter 1 and 2. Based on the City's flow control map, this site falls within the Peak Flow Control Duration Standard, Existing Conditions. This project is subject to full drainage review. The applicant submitted a Preliminary Drainage Report prepared by Sitewise Design, dated June 9, 2016 (Exhibit 7).</p> <p>The report also includes a detailed summary of the pre and post developed conditions. Stormwater runoff from the proposed improvements would be collected via roof drains, catch basins, and area drains. Runoff would be routed to a duplex pump chamber and would be discharged to the existing storm main in the SPU right-of-way to the north of the project site. Connection to the existing storm drainage system in Whitworth Ave S is preferred over a connection to the existing storm drainage system in the SPU right-of-way. Work in the right-of-way may require additional permit and coordination with Seattle Public Utilities and the City of Seattle. Therefore, staff recommends a condition of approval requiring the applicant provide the City with SPU authorization to construct infrastructure improvements within the SPU right-of-way before construction permits are issued.</p> <p>Flow control is not required as the project will create less than a 0.1 cfs increase in the</p>

existing site condition's 100 year peak flow. Water quality is also not required as the project will create less than 5,000 SF of new and replaced pollution generated impervious surfaces. The report includes a proposal for the use of a vegetated roof and permeable pavement for the flow control BMP. Required flow control BMPs shall be in accordance with Section 1.2.3.3 and Section 5.2 of the 2009 KCSWDM.

#### **Transportation.**

Staff Comment: The applicant submitted a Traffic Impact Analysis prepared by Gibson Traffic Consultants, Inc., dated April 18, 2016 (Exhibit 8). The provided TIA was found to meet the intent of the TIA guidelines and is generally acceptable for preliminary review.

Level of Service: It is anticipated that the proposed development would generate approximately 201 average daily trips with 14 new AM peak-hour trips and 19 PM peak-hour trips. It is anticipated that trips generated by the Renton Commons development will split 50% traveling to and from the north and 50% traveling to and from the south. The development will impact off-site City of Renton intersections with less than 10 peak-hour trips impacting any public intersections. Therefore, additional analysis of impacts to surrounding intersections is not required based on City of Renton threshold requirements.

Increased traffic created by the development would be mitigated by payment of transportation impact fees. The transportation impact fee that is current at the time of building permit application will be levied. The fee currently being assessed, in 2016, is \$1,923.83 per new multi-family unit. The fee is estimated at approximately \$92,343.84. The fee shall be payable to the City at the time of building permit issuance.

Street Improvements: Street Improvements are regulated by RMC 4-6-060 – Street Standards. See below:

Whitworth Ave S – Whitworth Ave S is classified as a residential access street. The existing right-of-way width is 60-feet. This portion of Whitworth Ave S falls within the City's downtown planning area. The City's Downtown Streetscape Design Standards and Guidelines apply. Street improvements fronting this site would be required to include a new 12-foot sidewalk, street trees (4-foot x 8-foot grates) behind the curb, and street lighting meeting City's arterial street lighting levels. Additionally, a 5-foot wide bike lane is required. The applicant is proposing to maintain the existing right-of-way and as a result the applicant has requested a modification to remove the 5-foot bike lane requirement.

Pursuant to RMC 4-9-250D the applicant is requesting an Administrative Modification from RMC 4-6-060, Street Standards in order to modify the cross section for Whitworth Ave S.

Staff is in support of the requested modification if all conditions of approval are met. See discussion below in FOF 30: Street Modification Analysis.

Concurrency: Transportation concurrency approval is not required given the proposal would have less than 20 peak hour trips.

#### **I. Schools:**

It is anticipated that the Renton School District can accommodate any additional students generated by this proposal at the following schools: Bryn Mawr Elementary (2.1 miles from the subject site), Dimmitt Middle School (2.0 miles from the subject

	<p>site), and Renton High School (0.2 miles from the subject site).</p> <p>Renton High School is within walking distance of the subject site while Bryn Mawr Elementary and Dimmitt Middle schools would require future students to be transported via bus. The site is surrounded by public sidewalks which facilitate safe walking conditions to Renton High School and would likely provide safe walking conditions to future bus locations.</p> <p>A School Impact Fee, based on new multifamily unit, will be required in order to mitigate the proposal's potential impacts to Renton School District. The fee is payable to the City as specified by the Renton Municipal Code. Currently the fee is assessed at \$1,385.00 per multi-family unit.</p>
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- 27. Design District Review:** The project site is located within Design District 'A'. The following table contains project elements intended to comply with the standards of the Design District 'A' Standards and guidelines, as outlined in RMC 4-3-100.E:

Compliance	Design District Guideline and Standard Analysis
	<p><b>1. SITE DESIGN AND BUILDING LOCATION:</b></p> <p><b>Intent:</b> To ensure that buildings are located in relation to streets and other buildings so that the Vision of the City of Renton can be realized for a high-density urban environment; so that businesses enjoy visibility from public rights-of-way; and to encourage pedestrian activity.</p> <p><b>a. Building Location and Orientation:</b></p> <p><b>Intent:</b> To ensure visibility of businesses and to establish active, lively uses along sidewalks and pedestrian pathways. To organize buildings for pedestrian use and so that natural light is available to other structures and open space. To ensure an appropriate transition between buildings, parking areas, and other land uses; and increase privacy for residential uses.</p> <p><b>Guidelines:</b> Developments shall enhance the mutual relationship of buildings with each other, as well as with the roads, open space, and pedestrian amenities while working to create a pedestrian oriented environment. Lots shall be configured to encourage variety and so that natural light is available to buildings and open space. The privacy of individuals in residential uses shall be provided for.</p>
<p><b>Compliant if Condition of Approval is Met</b></p>	<p><b>Standard:</b> The availability of natural light (both direct and reflected) and direct sun exposure to nearby buildings and open space (except parking areas) shall be considered when siting structures.</p> <p><u>Staff Comment:</u> The proposed structure is located on a majority of the site with little to no setbacks. The proposed building massing would be L-shaped with an outdoor play area and a vehicle access driveway located on the southeast corner of the site. The proposed design includes the availability of natural light into the building as well as natural sunlight to both the outdoor common spaces and the residential units above.</p> <p>However, with the absence of natural light is limited for the building's ground floor. The proposed indoor areas along the north, south and east elevations would benefit from the availability of additional natural light. This objective could be met with an increase in height for the ground level windows.</p> <p>Therefore, staff recommends a condition of approval requiring the applicant submit revised elevations which incorporate additional height for the ground level windows</p>

	<i>along the north, south, and eastern facades to a height of no less than 12 feet. The revised elevations shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</i>
✓	<b>Standard:</b> Buildings shall be oriented to the street with clear connections to the sidewalk.
✓	<b>Standard:</b> The front entry of a building shall be oriented to the street or a landscaped pedestrian-only courtyard.
N/A	<b>Standard:</b> Buildings with residential uses located at the street level shall be: <ol style="list-style-type: none"> <li>Set back from the sidewalk a minimum of ten feet (10') and feature substantial landscaping between the sidewalk and the building; or</li> <li>Have the ground floor residential uses raised above street level for residents' privacy.</li> </ol>
<b>b. Building Entries:</b>  <b>Intent:</b> To make building entrances convenient to locate and easy to access, and ensure that building entries further the pedestrian nature of the fronting sidewalk and the urban character of the district.  <b>Guidelines:</b> Primary entries shall face the street, serve as a focal point, and allow space for social interaction. All entries shall include features that make them easily identifiable while reflecting the architectural character of the building. The primary entry shall be the most visually prominent entry. Pedestrian access to the building from the sidewalk, parking lots, and/or other areas shall be provided and shall enhance the overall quality of the pedestrian experience on the site.	
✓	<b>Standard:</b> A primary entrance of each building shall be located on the facade facing a street, shall be prominent, visible from the street, connected by a walkway to the public sidewalk, and include human-scale elements.
✓	<b>Standard:</b> A primary entrance of each building shall be made visibly prominent by incorporating architectural features such as a facade overhang, trellis, large entry doors, and/or ornamental lighting.
Compliant if Condition of Approval is Met	<b>Standard:</b> Building entries from a street shall be clearly marked with canopies, architectural elements, ornamental lighting, or landscaping and include weather protection at least four and one-half feet (4-1/2') wide (illustration below). Buildings that are taller than thirty feet (30') in height shall also ensure that the weather protection is proportional to the distance above ground level.  <u>Staff Comment:</u> <i>The main entrance is centrally located along the eastern (Whitworth Ave S) façade of the structure and proposed to be marked with signage and a canopy structure providing overhead weather protection.</i>  <i>Currently the height of the bottom of the canopy is shown at approximately 13 feet above grade. Increasing the height of the entry canopy would serve to emphasize the project's taller ground floor base, as well as ensure added clearance for pedestrians passing under it as part of the requested encroachment over the right-of-way. Therefore, staff recommends a condition of approval requiring the canopy along southern and eastern facades be increased to a height of no less than 14 feet at the bottom of the canopy. Depending upon the final design, the canopy should extend 15-feet above grade. Designing the canopy to either: tilt upward, reduce the width of the face of the canopy, or modulate consistent with the bay window encroachment above would also help emphasize a taller ground floor height especially at the entrance. The revised elevations shall be submitted to, and approved by, the Current</i>

	<p><i>Planning Project Manager prior to building permit approval.</i></p> <p><i>Staff has recommended the applicant submit revised elevations with additional glazing which would serve to enhance the natural light to the ground floor (See discussion above). The recommended condition would also serve to amplify the entry.</i></p>
✓	<b>Standard:</b> Building entries from a parking lot shall be subordinate to those related to the street.
✓	<b>Standard:</b> Features such as entries, lobbies, and display windows shall be oriented to a street or pedestrian-oriented space; otherwise, screening or decorative features should be incorporated.
N/A	<b>Standard:</b> Multiple buildings on the same site shall direct views to building entries by providing a continuous network of pedestrian paths and open spaces that incorporate landscaping.
N/A	<b>Standard:</b> Ground floor residential units that are directly accessible from the street shall include entries from front yards to provide transition space from the street or entries from an open space such as a courtyard or garden that is accessible from the street.
<p><b>c. Transition to Surrounding Development:</b></p> <p><b>Intent:</b> To shape redevelopment projects so that the character and value of Renton's long-established, existing neighborhoods are preserved.</p> <p><b>Guidelines:</b> Careful siting and design treatment shall be used to achieve a compatible transition where new buildings differ from surrounding development in terms of building height, bulk and scale.</p>	
✓	<p><b>Standard:</b> At least one of the following design elements shall be used to promote a transition to surrounding uses:</p> <ol style="list-style-type: none"> <li>1. Building proportions, including step-backs on upper levels in accordance with the surrounding planned and existing land use forms; or</li> <li>2. Building articulation to divide a larger architectural element into smaller increments; or</li> <li>3. Roof lines, roof pitches, and roof shapes designed to reduce apparent bulk and transition with existing development.</li> </ol> <p>Additionally, the Administrator may require increased setbacks at the side or rear of a building in order to reduce the bulk and scale of larger buildings and/or so that sunlight reaches adjacent and/or abutting yards.</p>
<p><b>d. Service Element Location and Design:</b></p> <p><b>Intent:</b> To reduce the potential negative impacts of service elements (i.e., waste receptacles, loading docks) by locating service and loading areas away from high-volume pedestrian areas, and screening them from view in high visibility areas.</p> <p><b>Guidelines:</b> Service elements shall be concentrated and located so that impacts to pedestrians and other abutting uses are minimized. The impacts of service elements shall be mitigated with landscaping and an enclosure with fencing that is made of quality materials.</p>	
✓	<b>Standard:</b> Service elements shall be located and designed to minimize the impacts on the pedestrian environment and adjacent uses. Service elements shall be concentrated and located where they are accessible to service vehicles and

	convenient for tenant use.
✓	<b>Standard:</b> In addition to standard enclosure requirements, garbage, recycling collection, and utility areas shall be enclosed on all sides, including the roof and screened around their perimeter by a wall or fence and have self-closing doors.
N/A	<b>Standard:</b> Service enclosures shall be made of masonry, ornamental metal or wood, or some combination of the three (3).
N/A	<b>Standard:</b> If the service area is adjacent to a street, pathway, or pedestrian-oriented space, a landscaped planting strip, minimum 3 feet wide, shall be located on 3 sides of such facility.
<b>2. PARKING AND VEHICULAR ACCESS:</b>	
<b>Intent:</b> To provide safe, convenient access to the Urban Center and the Center Village; incorporate various modes of transportation, including public mass transit, in order to reduce traffic volumes and other impacts from vehicles; ensure sufficient parking is provided, while encouraging creativity in reducing the impacts of parking areas; allow an active pedestrian environment by maintaining contiguous street frontages, without parking lot siting along sidewalks and building facades; minimize the visual impact of parking lots; and use access streets and parking to maintain an urban edge to the district.	
<b>a. Surface Parking:</b>	
<b>Intent:</b> To maintain active pedestrian environments along streets by placing parking lots primarily in back of buildings.	
<b>Guidelines:</b> Surface parking shall be located and designed so as to reduce the visual impact of the parking area and associated vehicles. Large areas of surface parking shall also be designed to accommodate future infill development.	
N/A	<b>Standard:</b> Parking shall be located so that no surface parking is located between:  (a) A building and the front property line; and/or  (b) A building and the side property line (when on a corner lot).
N/A	<b>Standard:</b> Parking shall be located so that it is screened from surrounding streets by buildings, landscaping, and/or gateway features as dictated by location.
<b>b. Structured Parking Garages:</b>	
<b>Intent:</b> To promote more efficient use of land needed for vehicle parking; encourage the use of structured parking; physically and visually integrate parking garages with other uses; and reduce the overall impact of parking garages.	
<b>Guidelines:</b> Parking garages shall not dominate the streetscape; they shall be designed to be complementary with adjacent and abutting buildings. They shall be sited to complement, not subordinate, pedestrian entries. Similar forms, materials, and/or details to the primary building(s) should be used to enhance garages.	
✓	<b>Standard:</b> Parking structures shall provide space for ground floor commercial uses along street frontages at a minimum of seventy five percent (75%) of the building frontage width.
N/A	<b>Standard:</b> The entire facade must feature a pedestrian-oriented facade. The Administrator of the Department of Community and Economic Development may approve parking structures that do not feature a pedestrian orientation in limited

	circumstances. If allowed, the structure shall be set back at least six feet (6') from the sidewalk and feature substantial landscaping. This landscaping shall include a combination of evergreen and deciduous trees, shrubs, and ground cover. This setback shall be increased to ten feet (10') when abutting a primary arterial and/or minor arterial.
N/A	<b>Standard:</b> Public facing facades shall be articulated by arches, lintels, masonry trim, or other architectural elements and/or materials.
✓	<b>Standard:</b> The entry to the parking garage shall be located away from the primary street, to either the side or rear of the building.
✓	<b>Standard:</b> Parking garages at grade shall include screening or be enclosed from view with treatment such as walls, decorative grilles, trellis with landscaping, or a combination of treatments.
N/A	<p><b>Standard:</b> The Administrator of the Department of Community and Economic Development or designee may allow a reduced setback where the applicant can successfully demonstrate that the landscaped area and/or other design treatment meets the intent of these standards and guidelines. Possible treatments to reduce the setback include landscaping components plus one or more of the following integrated with the architectural design of the building:</p> <ul style="list-style-type: none"> <li>(a) Ornamental grillwork (other than vertical bars);</li> <li>(b) Decorative artwork;</li> <li>(c) Display windows;</li> <li>(d) Brick, tile, or stone;</li> <li>(e) Pre-cast decorative panels;</li> <li>(f) Vine-covered trellis;</li> <li>(g) Raised landscaping beds with decorative materials; or</li> <li>(h) Other treatments that meet the intent of this standard...</li> </ul>
<b>c. Vehicular Access:</b>  <b>Intent:</b> To maintain a contiguous and uninterrupted sidewalk by minimizing, consolidating, and/or eliminating vehicular access off streets.  <b>Guidelines:</b> Vehicular access to parking garages and parking lots shall not impede or interrupt pedestrian mobility. The impacts of curb cuts to pedestrian access on sidewalks shall be minimized.	
Compliant with Guideline	<p><b>Standard:</b> Access to parking lots and garages shall be from alleys, when available. If not available, access shall occur at side streets.</p> <p><b>Staff Comment:</b> Access to the site is proposed via a new single curb cut extended from Whitworth Ave S. While there is an alley abutting the property to the west, it is heavily encumbered by existing structures which limit access. The single proposed access point would minimize the impacts to pedestrian access on the adjacent sidewalk.</p>
✓	<b>Standard:</b> The number of driveways and curb cuts shall be minimized, so that pedestrian circulation along the sidewalk is minimally impeded.
<b>3. PEDESTRIAN ENVIRONMENT:</b>  <b>Intent:</b> To enhance the urban character of development in the Urban Center and the Center Village	

by creating pedestrian networks and by providing strong links from streets and drives to building entrances; make the pedestrian environment safer and more convenient, comfortable, and pleasant to walk between businesses, on sidewalks, to and from access points, and through parking lots; and promote the use of multi-modal and public transportation systems in order to reduce other vehicular traffic.

**a. Pedestrian Circulation:**

**Intent:** To create a network of linkages for pedestrians to improve safety and convenience and enhance the pedestrian environment.

**Guidelines:** The pedestrian environment shall be given priority and importance in the design of projects. Sidewalks and/or pathways shall be provided and shall provide safe access to buildings from parking areas. Providing pedestrian connections to abutting properties is an important aspect of connectivity and encourages pedestrian activity and shall be considered. Pathways shall be easily identifiable to pedestrians and drivers.

✓	<p><b>Standard:</b> A pedestrian circulation system of pathways that are clearly delineated and connect buildings, open space, and parking areas with the sidewalk system and abutting properties shall be provided.</p> <p>(a) Pathways shall be located so that there are clear sight lines, to increase safety.</p> <p>(b) Pathways shall be an all-weather or permeable walking surface, unless the applicant can demonstrate that the proposed surface is appropriate for the anticipated number of users and complementary to the design of the development.</p>
✓	<p><b>Standard:</b> Pathways within parking areas shall be provided and differentiated by material or texture (i.e., raised walkway, stamped concrete, or pavers) from abutting paving materials. Permeable materials are encouraged. The pathways shall be perpendicular to the applicable building facade and no greater than one hundred fifty feet (150') apart.</p>
✓	<p><b>Standard:</b> Sidewalks and pathways along the facades of buildings shall be of sufficient width to accommodate anticipated numbers of users. Specifically:</p> <p>(a) Sidewalks and pathways along the facades of mixed use and retail buildings 100 or more feet in width (measured along the facade) shall provide sidewalks at least 12 feet in width. The walkway shall include an 8 foot minimum unobstructed walking surface.</p> <p>(b) Interior pathways shall be provided and shall vary in width to establish a hierarchy. The widths shall be based on the intended number of users; to be no smaller than five feet (5') and no greater than twelve feet (12').</p> <p>(c) For all other interior pathways, the proposed walkway shall be of sufficient width to accommodate the anticipated number of users.</p>
N/A	<p><b>Standard:</b> Mid-block connections between buildings shall be provided.</p>

**4. RECREATION AREAS AND COMMON OPEN SPACE:**

**Intent:** To ensure that areas for both passive and active recreation are available to residents, workers, and visitors and that these areas are of sufficient size for the intended activity and in convenient locations. To create usable and inviting open space that is accessible to the public; and to promote pedestrian activity on streets particularly at street corners.

**Guidelines:** Developments located at street intersections should provide pedestrian-oriented space

at the street corner to emphasize pedestrian activity (illustration below). Recreation and common open space areas are integral aspects of quality development that encourage pedestrians and users. These areas shall be provided in an amount that is adequate to be functional and usable; they shall also be landscaped and located so that they are appealing to users and pedestrians

✓	<p><b>Standard:</b> All mixed use residential and attached housing developments of ten (10) or more dwelling units shall provide common open space and/or recreation areas.</p> <p>(a) At minimum, fifty (50) square feet per unit shall be provided.</p> <p>(b) The location, layout, and proposed type of common space or recreation area shall be subject to approval by the Administrator.</p> <p>(c) Open space or recreation areas shall be located to provide sun and light exposure to the area and located so that they are aggregated to provide usable area(s) for residents.</p> <p>(d) At least one of the following shall be provided in each open space and/or recreation area (the Administrator may require more than one of the following elements for developments having more than one hundred (100) units):</p> <ul style="list-style-type: none"> <li>i. Courtyards, plazas, pea patches, or multi-purpose open spaces;</li> <li>ii. Upper level common decks, patios, terraces, or roof gardens. Such spaces above the street level must feature views or amenities that are unique to the site and are provided as an asset to the development;</li> <li>iii. Pedestrian corridors dedicated to passive recreation and separate from the public street system;</li> <li>iv. Recreation facilities including, but not limited to, tennis/sports courts, swimming pools, exercise areas, game rooms, or other similar facilities; or</li> <li>v. Children's play spaces that are centrally located near a majority of dwelling units and visible from surrounding units. They shall also be located away from hazardous areas such as garbage dumpsters, drainage facilities, and parking areas.</li> </ul> <p>(e) The following shall not be counted toward the common open space or recreation area requirement:</p> <ul style="list-style-type: none"> <li>i. Required landscaping, driveways, parking, or other vehicular use areas.</li> <li>ii. Required yard setback areas. Except for areas that are developed as private or semi-private (from abutting or adjacent properties) courtyards, plazas or passive use areas containing landscaping and fencing sufficient to create a fully usable area accessible to all residents of the development.</li> <li>iii. Private decks, balconies, and private ground floor open space.</li> <li>iv. Other required landscaping and sensitive area buffers without common access links, such as pedestrian trails.</li> </ul>
N/A	<p><b>Standard:</b> All buildings and developments with over thirty thousand (30,000) square feet of nonresidential uses (excludes parking garage floorplate areas) shall provide pedestrian-oriented space....</p>
N/A	<p><b>Standard:</b> Public plazas shall be provided at intersections identified in the Commercial Arterial Zone Public Plaza Locations Map and as listed below:</p>

	(a) Benson Area....
N/A	<b>Standard:</b> The plaza shall measure no less than one thousand (1,000) square feet with a minimum dimension of twenty feet (20') on one side abutting the sidewalk.
N/A	<b>Standard:</b> The public plaza must be landscaped consistent with RMC <u>4-4-070</u> , including at minimum street trees, decorative paving, pedestrian-scaled lighting, and seating.
<b>5. BUILDING ARCHITECTURAL DESIGN:</b>	
<b>Intent:</b> To encourage building design that is unique and urban in character, comfortable on a human scale, and uses appropriate building materials that are suitable for the Pacific Northwest climate. To discourage franchise retail architecture.	
<b>a. Building Character and Massing:</b>	
<b>Intent:</b> To ensure that buildings are not bland and visually appear to be at a human scale; and ensure that all sides of a building, that can be seen by the public, are visually interesting.	
<b>Guidelines:</b> Building facades shall be modulated and/or articulated to reduce the apparent size of buildings, break up long blank walls, add visual interest, and enhance the character of the neighborhood. Articulation, modulation, and their intervals should create a sense of scale important to residential buildings.	
✓	<b>Standard:</b> All building facades shall include modulation or articulation at intervals of no more than forty feet (40').
✓	<b>Standard:</b> Modulations shall be a minimum of two feet (2') deep, sixteen feet (16') in height, and eight feet (8') in width.
N/A	<b>Standard:</b> Buildings greater than one hundred sixty feet (160') in length shall provide a variety of modulations and articulations to reduce the apparent bulk and scale of the facade (illustration in District B, below); or provide an additional special feature such as a clock tower, courtyard, fountain, or public gathering area.
<b>b. Ground-Level Details:</b>	
<b>Intent:</b> To ensure that buildings are visually interesting and reinforce the intended human-scale character of the pedestrian environment; and ensure that all sides of a building within near or distant public view have visual interest.	
<b>Guidelines:</b> The use of material variations such as colors, brick, shingles, stucco, and horizontal wood siding is encouraged. The primary building entrance should be made visibly prominent by incorporating architectural features such as a facade overhang, trellis, large entry doors, and/or ornamental lighting (illustration below). Detail features should also be used, to include things such as decorative entry paving, street furniture (benches, etc.), and/or public art.	
<b>Compliant if Condition of Approval is Met</b>	<p><b>Standard:</b> Human-scaled elements such as a lighting fixture, trellis, or other landscape feature shall be provided along the facade's ground floor.</p> <p><b>Staff Comment:</b> While the applicant notes human-scaled elements such as lighting fixtures or other landscape features along the majority of the building's southern and/or eastern façades are proposed, the proposal would benefit from additional decorative details along the ground façade to ensure the human scale character of the pedestrian environment is reinforced especially given the requested increases in density. A recommended condition above for compliance with the Downtown Streetscape Design Standards and Guidelines would partially meet the intent of this standard (see FOF 25: Zoning Development Standard Compliance, Landscaping). The</p>

	<i>inclusion of public art along the frontage of the site would further serve to comply with the Ground Level Detail standard. Therefore, staff recommends a condition of approval requiring the applicant consider adding public artwork, along Whitworth Ave S, that is publicly visible and in keeping with the vision of Downtown Renton as an arts center. A narrative regarding the ability to incorporate public art into the proposal shall be submitted to the Current Planning Project Manager prior to building permit approval. If appropriate, public art shall be included in a revised landscape plan to be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</i>
✓	<b>Standard:</b> On any facade visible to the public, transparent windows and/or doors are required to comprise at least 50 percent of the portion of the ground floor facade that is between 4 feet and 8 feet above ground (as measured on the true elevation).
✓	<b>Standard:</b> Upper portions of building facades shall have clear windows with visibility into and out of the building. However, screening may be applied to provide shade and energy efficiency. The minimum amount of light transmittance for windows shall be 50 percent.
N/A	<b>Standard:</b> Display windows shall be designed for frequent change of merchandise, rather than permanent displays.
✓	<b>Standard:</b> Where windows or storefronts occur, they must principally contain clear glazing.
✓	<b>Standard:</b> Tinted and dark glass, highly reflective (mirror-type) glass and film are prohibited.
✓	<p><b>Standard:</b> Untreated blank walls visible from public streets, sidewalks, or interior pedestrian pathways are prohibited. A wall (including building facades and retaining walls) is considered a blank wall if:</p> <p>(a) It is a ground floor wall or portion of a ground floor wall over 6 feet in height, has a horizontal length greater than 15 feet, and does not include a window, door, building modulation or other architectural detailing; or</p> <p>(b) Any portion of a ground floor wall has a surface area of 400 square feet or greater and does not include a window, door, building modulation or other architectural detailing.</p>
N/A	<p><b>Standard:</b> If blank walls are required or unavoidable, blank walls shall be treated with one or more of the following:</p> <p>(a) A planting bed at least five feet in width containing trees, shrubs, evergreen ground cover, or vines adjacent to the blank wall;</p> <p>(b) Trellis or other vine supports with evergreen climbing vines;</p> <p>(c) Architectural detailing such as reveals, contrasting materials, or other special detailing that meets the intent of this standard;</p> <p>(d) Artwork, such as bas-relief sculpture, mural, or similar; or</p> <p>(e) Seating area with special paving and seasonal planting.</p>
<b>c. Building Roof Lines:</b>	
<b>Intent:</b> To ensure that roof forms provide distinctive profiles and interest consistent with an urban	

project and contribute to the visual continuity of the district.

**Guidelines:** Building roof lines shall be varied and include architectural elements to add visual interest to the building.

✓	<p><b>Standard:</b> Buildings shall use at least one of the following elements to create varied and interesting roof profiles:</p> <ul style="list-style-type: none"> <li>(a) Extended parapets;</li> <li>(b) Feature elements projecting above parapets;</li> <li>(c) Projected cornices;</li> <li>(d) Pitched or sloped roofs</li> <li>(e) Buildings containing predominantly residential uses shall have pitched roofs with a minimum slope of one to four (1:4) and shall have dormers or interesting roof forms that break up the massiveness of an uninterrupted sloping roof.</li> </ul>
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**d. Building Materials:**

**Intent:** To ensure high standards of quality and effective maintenance over time; encourage the use of materials that reduce the visual bulk of large buildings; and encourage the use of materials that add visual interest to the neighborhood.

**Guidelines:** Building materials are an important and integral part of the architectural design of a building that is attractive and of high quality. Material variation shall be used to create visual appeal and eliminate monotony of facades. This shall occur on all facades in a consistent manner. High quality materials shall be used. If materials like concrete or block walls are used they shall be enhanced to create variation and enhance their visual appeal.

✓	<p><b>Standard:</b> All sides of buildings visible from a street, pathway, parking area, or open space shall be finished on all sides with the same building materials, detailing, and color scheme, or if different, with materials of the same quality.</p>
✓	<p><b>Standard:</b> All buildings shall use material variations such as colors, brick or metal banding, patterns or textural changes.</p>
✓	<p><b>Standard:</b> Materials, individually or in combination, shall have texture, pattern, and be detailed on all visible facades.</p>
<b>Compliant if Condition of Approval is Met</b>	<p><b>Standard:</b> Materials shall be durable, high quality, and consistent with more traditional urban development, such as brick, integrally colored concrete masonry, pre-finished metal, stone, steel, glass and cast-in-place concrete.</p> <p><i>Staff Comment:</i> The applicant has proposed a building exterior with varied colors, textures, and profiles. It isn't clear if the proposal would use high quality building materials. In order to ensure that quality materials are used staff recommends the applicant submit a materials board subject to the approval of the Current Planning Project Manager prior to building permit approval. Acceptable materials include a combination of brick, integrally colored concrete masonry, pre-finished metal, stone, steel, glass, cast-in-place concrete, or other high quality material. The materials board shall also include, but not be limited to the following: street level windows; frames and glass, cedar siding strips, any proposed fencing (especially surrounding the playground), trellis/pergola structure on south side of property, bicycle rack canopy, and parapet cap. Any non-brick masonry finishes proposed at the ground level that</p>

	<p>may be accessible to humans shall be anti-graffiti coated to ensure easy removal of graffiti.</p> <p>If this condition of approval is met the proposal would satisfy this standard.</p>
✓	<p><b>Standard:</b> If concrete is used, walls shall be enhanced by techniques such as texturing, reveals, and/or coloring with a concrete coating or admixture.</p>
✓	<p><b>Standard:</b> If concrete block walls are used, they shall be enhanced with integral color, textured blocks and colored mortar, decorative bond pattern and/or shall incorporate other masonry materials.</p>
✓	<p><b>Standard:</b> All buildings shall use material variations such as colors, brick or metal banding, patterns, or textural changes.</p>
<p><b>6. SIGNAGE:</b> In addition to the City's standard sign regulations, developments within Urban Design District A are also subject to the additional sign restrictions found in RMC 4-4-100G, urban design sign area regulations. Modifications to the standard requirements found in RMC 4-4-100G are possible for those proposals that can comply with the Design District criteria found in RMC 4-3-100F, Modification of Minimum Standards. For proposals unable to meet the modification criteria, a variance is required.</p>	
Compliant if Condition of Approval is Met	<p><b>Standard:</b> Signage shall be an integral part of the design approach to the building.</p> <p><u>Staff Comment:</u> Signage has not yet been designed for the proposed project. The applicant would be required to submit a comprehensive signage package which complies with the standards of Design District 'A'. However to ensure the proposed signage in-keeping with the architectural character of the structure staff recommends a conceptual sign package, which indicates approximate locations of all exterior building signage be submitted. Locations and supports are required to be compatible with the building's architecture and exterior finishes. The conceptual sign package shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</p>
N/A	<p><b>Standard:</b> In mixed use and multi-use buildings, signage shall be coordinated with the overall building design.</p>
N/A	<p><b>Standard:</b> Corporate logos and signs shall be sized appropriately for their location.</p>
Compliant if Condition of Approval is Met	<p><b>Standard:</b> Entry signs shall be limited to the name of the larger development.</p> <p><u>Staff Comment:</u> See condition above.</p>
N/A	<p><b>Standard:</b> Alteration of trademarks notwithstanding, corporate signage should not be garish in color nor overly lit, although creative design, strong accent colors, and interesting surface materials and lighting techniques are encouraged.</p>
Compliant if Condition of Approval is Met	<p><b>Standard:</b> Front-lit, ground-mounted monument signs are the preferred type of freestanding sign.</p> <p><u>Staff Comment:</u> See condition above.</p>
Compliant if Condition of Approval is Met	<p><b>Standard:</b> Blade type signs, proportional to the building facade on which they are mounted, are encouraged on pedestrian-oriented streets.</p> <p><u>Staff Comment:</u> See condition above.</p>
<p><b>6. LIGHTING:</b></p>	

<b>Intent:</b> To ensure safety and security; provide adequate lighting levels in pedestrian areas such as plazas, pedestrian walkways, parking areas, building entries, and other public places; and increase the visual attractiveness of the area at all times of the day and night.	
<b>Guidelines:</b> Lighting that improves pedestrian safety and also that creates visual interest in the building and site during the evening hours shall be provided.	
<b>Compliant if Condition of Approval is Met</b>	<b>Standard:</b> Pedestrian-scale lighting shall be provided at primary and secondary building entrances. Examples include sconces on building facades, awnings with down-lighting and decorative street lighting.  <b>Staff Comment:</b> The application includes a lighting plan which indicates that building lighting will be utilized to complement the architecture of the structure (Exhibit 14).  See additional discussion below in FOF 30: Street Modification Analysis
✓	<b>Standard:</b> Accent lighting shall also be provided on building facades (such as sconces) and/or to illuminate other key elements of the site such as gateways, specimen trees, other significant landscaping, water features, and/or artwork.
✓	<b>Standard:</b> Downlighting shall be used in all cases to assure safe pedestrian and vehicular movement, unless alternative pedestrian scale lighting has been approved administratively or is specifically listed as exempt from provisions located in RMC 4-4-075, Lighting, Exterior On-Site (i.e., signage, governmental flags, temporary holiday or decorative lighting, right-of-way-lighting, etc.).

- 28. Conditional Use Permit:** The proposal requires a Conditional Use Permit in order to establish increase the base density from 100 du/ac to 150 du/ac. The following table contains project elements intended to comply with Conditional Use Permit decision criteria as related to the request for an increase in density, as outlined in RMC 4-9-030.D:

Compliance	Conditional Use Permit Criteria and Analysis
<b>Compliant if Conditions of Approval Are Met</b>	<b>a. Consistency with Plans and Regulations:</b> The proposed use shall be compatible with the general goals, objectives, policies and standards of the Comprehensive Plan, the zoning regulations and any other plans, programs, maps or ordinances of the City of Renton.  <b>Staff Comment:</b> See FOF 24: Comprehensive Plan Compliance and FOF 25: Zoning Development Standard Compliance.
✓	<b>b. Appropriate Location:</b> The proposed location shall not result in the detrimental overconcentration of a particular use within the City or within the immediate area of the proposed use. The proposed location shall be suited for the proposed use.  <b>Staff Comment:</b> The issue of housing affordability has worsened over the previous years, with reports of more and more people paying as much as 50% of their monthly income for rent; rent burden is defined as paying more than 30%. Additionally, increases in individuals and families without a permanent address point to the benefit of building units to be part of the solution.  Locating added density in downtown and near transit hubs supports growth near transit and preserves the single-family communities of Renton while managing growth to accommodate rental housing demand.  The site at has some unique synergies. The adjacent Harambee Church provides community meals and resource referrals for those experiencing homelessness. The

	<p>church also houses the Cry Out! program for youth, giving access to music recording studio, dance, art, and various life skills workshops in robust afterschool programs for middle and high school students. The site is also very near Renton High School, and close to Safeway, the Renton Farmer's Market, and other shops and restaurants. As a result there are unique opportunities to provide working individuals and families, including those who have exited homelessness, access to these amenities.</p> <p>Increasing the density at this site by permitting the building of another 16 units over what is permitted outright, would bring housing within reach of an additional 16 households.</p> <p>Additionally, the proposed project would be located in the Downtown Core. The proposed architecture and streetscape amenities, if all conditions of approval are met, would enhance the character of the Downtown Core.</p>
✓	<p><b>c. Effect on Adjacent Properties:</b> The proposed use at the proposed location shall not result in substantial or undue adverse effects on adjacent property.</p> <p><u>Staff Comment:</u> The proposed project's increase in density is not anticipated to create undue adverse effects on surrounding properties if all conditions of approval are met. The project is expected to assist in revitalization efforts for the Downtown Core area.</p>
✓	<p><b>d. Compatibility:</b> The proposed use shall be compatible with the scale and character of the neighborhood.</p> <p><u>Staff Comment:</u> The surrounding neighborhood is in transition with some low-rise small-scale existing buildings and a growing number of multi-story multifamily and mixed-use buildings. The existing buildings around the site include a single family house to the west, churches to the south and east, and an office building for an international adoption agency to the north. A four-story mixed-use building, the Compass Veterans Center, is located to the northeast, diagonally across Whitworth Ave S. Renton High School is also located a block to the north.</p> <p>Although the proposed building would be taller than immediately surrounding buildings, the area is zoned CD and likely to change as incremental redevelopment occurs. Comprehensive Plan policies encourage taller buildings and more intensive use of properties in the downtown core. The CD zoning allows for a 95-foot height limit.</p> <p>Based on existing uses within the surrounding area, staff considers the proposal to be compatible with the neighborhood.</p>
<b>Compliant if Condition of Approval is Met</b>	<p><b>e. Parking:</b> Adequate parking is, or will be made, available.</p> <p><u>Staff Comment:</u> See discussion under FOF 25: Zoning and Development Standard Compliance, Parking.</p>
<b>Compliant if Condition of Approval is Met</b>	<p><b>f. Traffic:</b> The use shall ensure safe movement for vehicles and pedestrians and shall mitigate potential effects on the surrounding area.</p> <p><u>Staff Comment:</u> See discussion under FOF 26: Site Plan Review Analysis, Transportation.</p>
<b>Compliant if Condition of Approval is Met</b>	<p><b>g. Noise, Light and Glare:</b> Potential noise, light and glare impacts from the proposed use shall be evaluated and mitigated.</p> <p><u>Staff Comment:</u> See discussion under FOF 26: Site Plan Review Analysis.</p>
<b>Compliant if</b>	<p><b>h. Landscaping:</b> Landscaping shall be provided in all areas not occupied by buildings,</p>

<b>Condition of Approval is Met</b>	<p>paving, or critical areas. Additional landscaping may be required to buffer adjacent properties from potentially adverse effects of the proposed use.</p> <p><i>Staff Comment: See discussion under FOF 25: Zoning and Development Standard Compliance, Landscaping.</i></p>
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- 29. Bicycle Parking Modification Analysis:** Pursuant to RMC 4-9-250D the applicant is requesting an Administrative Modification from RMC 4-4-080, in order to locate bicycle parking outside of the structure. The proposal is compliant with the following modification criteria, pursuant to RMC 4 9 250D, if all conditions of approval are met:

Compliance	Modification Criteria and Analysis
✓	<p>a. Substantially implements the policy direction of the policies and objectives of the Comprehensive Plan Land Use Element and the Community Design Element and the proposed modification is the minimum adjustment necessary to implement these policies and objectives.</p> <p><i>Staff Comment: See FOF 24, Comprehensive Plan Analysis.</i></p>
<b>Compliant if Condition of Approval is Met</b>	<p>b. Will meet the objectives and safety, function, appearance, environmental protection and maintainability intended by the Code requirements, based upon sound engineering judgment.</p> <p><i>Staff Comment: The site plan is showing varied locations for 24 bicycle spaces exterior to the structure. The applicant contends in LIHI's other developments, that people occupying studios and one-bedrooms as individuals or couples and young families who are bike riders and bike commuters, prefer to store their bicycles within their units. As a result the applicant is proposing to provide a wall-mounted bike rack for those who request it in order to addresses the need for bike storage which is secure.</i></p> <p><i>The bike rack location along the northern property line would be very isolated from view from any transitory traffic, which would potentially make bicycles susceptible to theft. Bicycles are commonly stolen either for transportation or to pawn at area pawn shops. Therefore, staff recommends the site plan be revised to remove and/or relocate the proposed bicycle rack currently proposed along the northern property line to a more visible location. The revised site plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.</i></p> <p><i>Staff concurs the proposed modification would meet the objectives of function and maintainability intended by the code requirement if all conditions of approval are met.</i></p>
✓	<p>c. Will not be injurious to other property(ies) in the vicinity.</p> <p><i>Staff Comment: The proposal includes adequate bicycle parking spaces for the intended use, along with special accommodations for secure bicycle parking on site. Therefore the proposed modification would not have impacts on surrounding properties within the vicinity of the subject site.</i></p>
✓	<p>d. Conforms to the intent and purpose of the Code.</p> <p><i>Staff Comment: See comments under criterion 'b'.</i></p>
✓	<p>e. Can be shown to be justified and required for the use and situation intended; and</p> <p><i>Staff Comment: See comments under criterion 'b'.</i></p>
✓	<p>f. Will not create adverse impacts to other property(ies) in the vicinity.</p>

	<u>Staff Comment:</u> See comments under criterion 'c'.
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- 30. Street Modification Analysis:** All frontage roads are required to meet street standards pursuant to RMC 4-6-060. The applicant is requesting a street modification, from RMC 4-6-060, in order to modify the requirement for required right-of-way improvements along Whitworth Ave S to eliminate the required 5-foot wide bicycle lane. The proposal is compliant with the following modification criteria, pursuant to RMC 4 9 250D, if all conditions of approval are met.:

Compliance	Modification Criteria and Analysis
✓	<p>a. Substantially implements the policy direction of the policies and objectives of the Comprehensive Plan Land Use Element and the Community Design Element and the proposed modification is the minimum adjustment necessary to implement these policies and objectives.</p> <p><u>Staff Comment:</u> See FOF 24: Comprehensive Plan Analysis.</p>
Compliant if Condition of Approval is Met	<p>b. Will meet the objectives and safety, function, appearance, environmental protection and maintainability intended by the Code requirements, based upon sound engineering judgment.</p> <p><u>Staff Comment:</u> The adopted City Center Community Plan requires that a 5-foot bike lane, curb and gutter, and 12 foot wide sidewalks be provided on both sides of the street (Exhibit 13). The addition of the 5 foot bike lane, revised location of curb and gutter, and 12-foot sidewalk along the project frontage would require a right of way dedication of 7-feet and a relocation of the existing curb.</p> <p>However Shattuck Ave S, the roadway just east of Whitworth Ave S, has been chosen as the designated appropriate bicycle commuter route per the City of Renton Bike and Trails Master Plan. Shattuck Avenue S provides the link connections to transit stops.</p> <p>Therefore, the City's Transportation Department determined Whitworth Ave S should maintain the curb traveled width of 36 feet. Staff has recommended a condition of approval requiring up to a two-foot wide pedestrian easement immediately adjacent to the right-of-way to accommodate the proposed 12-foot wide sidewalk (see discussion under FOF 26: Site Plan Review Analysis, Circulation). However, staff recommends as a condition of approval requiring the replacement of the existing curb along the frontage of the site. A revised paving plan shall be submitted to, and be approved by, the Plan Reviewer prior to construction permit approval.</p> <p>If all conditions of approval are met staff concurs the proposed modification would meet the objectives of function and maintainability intended by the code requirements.</p>
✓	<p>c. Will not be injurious to other property(ies) in the vicinity.</p> <p><u>Staff Comment:</u> Should the requested modification be approved it would not be injurious to other properties within the vicinity of the subject site. Shattuck Ave S, the roadway just east of Whitworth Ave S, has been chosen as the designated appropriate bicycle commuter route per the City of Renton Bike and Trails Master Plan. Shattuck Avenue S provides the link connections to transit stops.</p>
✓	<p>d. Conforms to the intent and purpose of the Code.</p> <p><u>Staff Comment:</u> See comments under criterion 'b'.</p>
✓	<p>e. Can be shown to be justified and required for the use and situation intended; and</p>

	<u>Staff Comment:</u> See comments under criterion 'b'.
✓	f. Will not create adverse impacts to other property(ies) in the vicinity. <u>Staff Comment:</u> See comments under criterion 'c'.

- 31. Refuse and Recycle Modification:** Pursuant to RMC 4-9-250D the applicant is requesting an Administrative Modification from RMC 4-4-090, Refuse and Recyclable Standards in order to reduce the number of collections points. The proposal is compliant with the following modification criteria, pursuant to RMC 4-9-250D, if all conditions of approval are met:

Compliance	Modification Criteria and Analysis
N/A	a. Substantially implements the policy direction of the policies and objectives of the Comprehensive Plan Land Use Element and the Community Design Element and the proposed modification is the minimum adjustment necessary to implement these policies and objectives. <u>Staff Comment:</u> The Comprehensive Plan does not specifically address refuse and recycle standards. Therefore this criterion is not applicable.
Compliant if Condition of Approval is Met	b. Will meet the objectives and safety, function, appearance, environmental protection and maintainability intended by the Code requirements, based upon sound engineering judgment. <u>Staff Comment:</u> The applicant contends the required number of deposit and collection locations would present a practical difficulty for creating a functional and efficient ground floor layout. If approved, the refuse deposit area would be located in a relatively centralized location at the rear of the building. It's location would also serve to provide ease of access to hauling trucks if all conditions of approval are complied with (see additional discussion under FOF 25: Zoning Development Standard Compliance, Refuse and Recyclables).  Staff concurs the requested modification conforms to the intent and purpose of the refuse and recyclable standards by providing adequate refuse deposit areas in the amount necessary for the Renton Commons project and sufficient locations as to not cause residents to travel very far to drop refuse and recyclables.
✓	c. Will not be injurious to other property(ies) in the vicinity. <u>Staff Comment:</u> The proposed refuse and recycle deposit areas would be located within the proposed structure and would therefore not have impacts on surrounding properties within the vicinity.
✓	d. Conforms to the intent and purpose of the Code. <u>Staff Comment:</u> See comments under criterion 'b'.
✓	e. Can be shown to be justified and required for the use and situation intended; and <u>Staff Comment:</u> See comments under criterion 'b'.
✓	f. Will not create adverse impacts to other property(ies) in the vicinity. <u>Staff Comment:</u> See comments under criterion 'c'.

**I. CONCLUSIONS:**

1. The subject site is located in the Commercial Mixed Use (CMU) Comprehensive Plan designation and complies with the goals and policies established with this designation if all conditions of approval are met, see FOF 24: Comprehensive Plan Analysis.
2. The subject site is located in the Center Downtown (CD) zoning designation and complies with the zoning and development standards established with this designation provided the applicant complies with City Code and conditions of approval, see FOF 25.
3. The proposal complies with the Urban Design Regulations, for District 'A' provided the applicant complies with City Code and conditions of approval, see FOF 27.
4. The proposal complies with the Site Plan Review regulations as established by City Code provided all advisory notes and conditions are complied with, see FOF 26.
5. The proposed density with the Conditional Use Permit regulations as established by City Code provided all advisory notes and conditions are complied with, see FOF 28.
6. The proposed bicycle parking modification from RMC 4-4-080, is compliant with criteria pursuant to RMC 4-9-250D, in order to eliminate the need for bicycle parking within the structure, if all conditions of approval are met, see FOF 29.
7. The proposed street modification from RMC 4-6-060, is compliant with criteria pursuant to RMC 4-9-250D, in order to eliminate the five-foot wide bicycle lane, if all conditions of approval are met, see FOF 30.
8. The proposed refuse and recycle modification from RMC 4-4-090, is compliant with criteria pursuant to RMC 4-9-250D, in order to reduce the number of collection points, if all conditions of approval are met, see FOF 31.
9. There are adequate public services and facilities to accommodate the proposed development, see FOF 26.

**J. RECOMMENDATION:**

Staff recommends approval of the Renton Commons Site Plan Review, Conditional Use Permit, and three (3) modification requests, File No. LUA16-000425, as depicted in Exhibit 2, subject to the following conditions:

1. The applicant shall comply with the mitigation measure issued as part of the Determination of Non-Significance Mitigated, dated July 25, 2016.
2. The applicant shall obtain a permanent right-of-way use permit in order to encroach into right-of-way by no more than two –feet for the central bay. The right-of-way use permit shall be obtained prior to construction/building permit approval.
3. The applicant shall demonstrate compliance with the Part 77 Horizontal Surface Height Limit prior to construction/building permit approval.
4. The applicant shall be required to submit a revised landscape plan to the Current Planning Project Manager prior to building permit approval. The revised landscape plan shall include the following: compliance with the draft Downtown Streetscape Design Standards and Guidelines (DSDSG) (dated March 16, 2016) or as may be amended or approved prior to plans being submitted for building permits; specific detail for courtyard screening and furniture; the addition of one litter receptacle to the frontage improvements located as close to primary building entrance as possible; and landscape planters to denote pedestrian entry points.

5. The applicant shall provide a copy of a joint parking agreement for those parking stalls located off site. The joint parking agreement shall be submitted to the Current Planning Project Manager prior to building permit approval.
6. The provision of an ADA van accessible parking stall shall be required. The revised parking plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
7. The applicant shall be required to submit a recorded pedestrian easement in sufficient width (up to 2 feet) to construct required street improvements behind the existing curb. The easement shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval.
8. The applicant shall submit a revised paving plan which includes a suitable transition to the existing sidewalk, north and south of the subject property. The revised paving plan shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval.
9. The applicant shall identify an off-site staging area for construction. The area shall be within a reasonable distance from site to limit construction traffic to and from the site. Pedestrian paths to and from Renton High School should be carefully studied to ensure a well-coordinated, signed, and maintained traffic control plan. The traffic control plan shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval. Additionally, the applicant shall be required to notify surrounding property owners (within 300-foot radius of the site) in advance of the start of construction (no less than 30 days), and provide updates no less than quarterly during the construction period.
10. The applicant shall be required to submit a lighting analysis demonstrating compliance with RMC 4-6-060.I.3. The lighting analysis shall be submitted to, and approved by, the Plan Reviewer prior to construction permit approval.
11. The applicant shall provide a lighting/safety plan which includes the following: lighting within the covered garage during all hours; lighting to illuminate the alley; a limited access gate for the garage; a surveillance system installed in the covered garage to help deter incidents of crime and suspicious activity; and an emergency communications device within the covered garage. The lighting/safety plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
12. The applicant shall provide a pavement design for Whitworth Ave S. The pavement design shall be submitted to, and approved by, the Current Planning Project Manager prior to construction permit approval.
13. The applicant shall relocate the proposed pergola/gazebo closer to Whitworth Ave S for more natural visibility, possibly in front of classroom window/exit corridor to the north or an alternative location. A revised landscaping plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
14. The applicant shall provide the City with SPU authorization to construct infrastructure improvements within the SPU right-of-way before construction permits are issued.
15. The applicant shall submit revised elevations which incorporate additional height for the ground level windows along the north, south, and eastern facades to a height of no less than 12 feet. The revised elevations shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
16. The canopy along southern and eastern facades shall be increased to a height of no less than 14 feet at the bottom of the canopy. Depending upon the final design, the canopy should extend 15-feet above grade. Designing the canopy to either: tilt upward, reduce the width of the face of the canopy, or modulate consistent with the bay window encroachment above would also help emphasize a taller

ground floor height especially at the entrance. The revised elevations shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.

17. The applicant shall consider adding public artwork, along Whitworth Ave S, that is publicly visible and in keeping with the vision of Downtown Renton as an arts center. A narrative regarding the ability to incorporate public art into the proposal shall be submitted to the Current Planning Project Manager prior to building permit approval. If appropriate, public art shall be included in a revised landscape plan to be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
18. The applicant shall submit a materials board subject to the approval of the Current Planning Project Manager prior to building permit approval. Acceptable materials include a combination of brick, integrally colored concrete masonry, pre-finished metal, stone, steel, glass, cast-in-place concrete, or other high quality material. The materials board shall also include, but not be limited to the following: street level windows; frames and glass, cedar siding strips, any proposed fencing (especially surrounding the playground), trellis/pergola structure on south side of property, bicycle rack canopy, and parapet cap. Any non-brick masonry finishes proposed at the ground level that may be accessible to humans shall be anti-graffiti coated to ensure easy removal of graffiti.
19. A conceptual sign package, which indicates approximate locations of all exterior building signage shall be submitted. Locations and supports are required to be compatible with the building's architecture and exterior finishes. The conceptual sign package shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
20. The site plan shall be revised to remove and/or relocate the proposed bicycle rack currently proposed along the northern property line to a more visible location. The revised site plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
21. The applicant shall be required to replace the existing curb along the frontage of the site. A revised paving plan shall be submitted to, and be approved by, the Plan Reviewer prior to construction permit approval.



## EXHIBITS

**Project Name:**  
Renton Commons

**Project Number:**  
LUA16-000425

**Date of Hearing**

8/16/16

**Staff Contact**

Rocale Timmons  
Senior Planner

**Project Contact/Applicant**

Pamela Derry  
Tonkin Architecture  
204 First Ave  
Seattle, WA 98104

**Project Location**

215 Whitworth Ave S

**The following exhibits were entered into the record:**

- Exhibit 1      ERC Report
- Exhibit 2      Site Plan
- Exhibit 3      Landscape Plan
- Exhibit 4      Elevations
- Exhibit 5      Grading Plan
- Exhibit 6      Geotechnical Report, prepared by Geotech Consultants (dated July 21, 2015)
- Exhibit 7      Drainage Report, prepared by Site Wise Design (dated June 7, 2016)
- Exhibit 8      Traffic Impact Analysis, prepared by Gibson Traffic Consultants (dated April 18, 2016)
- Exhibit 9      Department of Archeology and Historic Preservation Comment letter (dated July 6, 2016)
- Exhibit 10      Preliminary Archaeological Assessment, prepared by Wilamette Cultural Resources (dated May 20, 2016)
- Exhibit 11:      Staff Recommendation to the Hearing Examiner (dated August 8, 2016)
- Exhibit 12:      SEPA Determination and Mitigation Measures (dated July 25, 2016)
- Exhibit 13:      City Center Community Plan
- Exhibit 14:      Lighting Plan
- Exhibit 15:      Downtown Streetscape Design Standards and Guidelines (DSDSG) (dated March 16, 2016)
- Exhibit 16:      Renderings

**ENVIRONMENTAL REVIEW COMMITTEE REPORT**

**ERC MEETING DATE:** July 25, 2016

**Project Name:** Renton Commons

**Project Number:** LUA16-000425, CU-H, SA-H, ECF, MOD, MOD, MOD

**Project Manager:** Rocale Timmons, Senior Planner

**Owner:** Low Income Housing Institute; 2407 First Ave; Seattle, WA 98121

**Contact:** Pamela Derry; Tonkin Architecture; 204 First Ave; Seattle, WA 98104

**Project Location:** 215 Whitworth Ave S

**Project Summary:** The Low Income Housing Institute (LIHI) is requesting Hearing Examiner Site Plan Review, Hearing Examiner Conditional Use Permit, Environmental 'SEPA' Review, and three modifications for the construction of a 6-story building containing 48 affordable multi-family residential units. The structure would have an average height of 64 feet. The 0.32 acre site is located within the Center Downtown (CD) zoning classification on the west side of Whitworth Ave S just north of S 3rd St at 215 Whitworth Ave S. Vehicular access to the site would be provided via a single entry point from Whitworth Ave S. A total of 12 parking spaces would be provided within the structure. A refuse and recycle modification, from RMC 4-4-090, is being requested in order to reduce the number of required deposit and collection points as well as the size requirements. A bicycle parking modification, from RMC 4-4-050, is being requested in order to reduce the number of required bicycle parking from 24 to 20 stalls. Finally, the applicant is requesting a street modification, from RMC 4-6-060, in order to maintain the existing 60-foot right-of-way, without dedication, and alter the required street cross section. The site is located within a Seismic Hazard Area. There appears to be no other critical areas located on site.

**Site Area:** 13,915 SF

**Total Building Area GSF:** 46,215 SF

**STAFF RECOMMENDATION:** Staff Recommends that the Environmental Review Committee issue a Determination of Non-Significance - Mitigated (DNS-M).



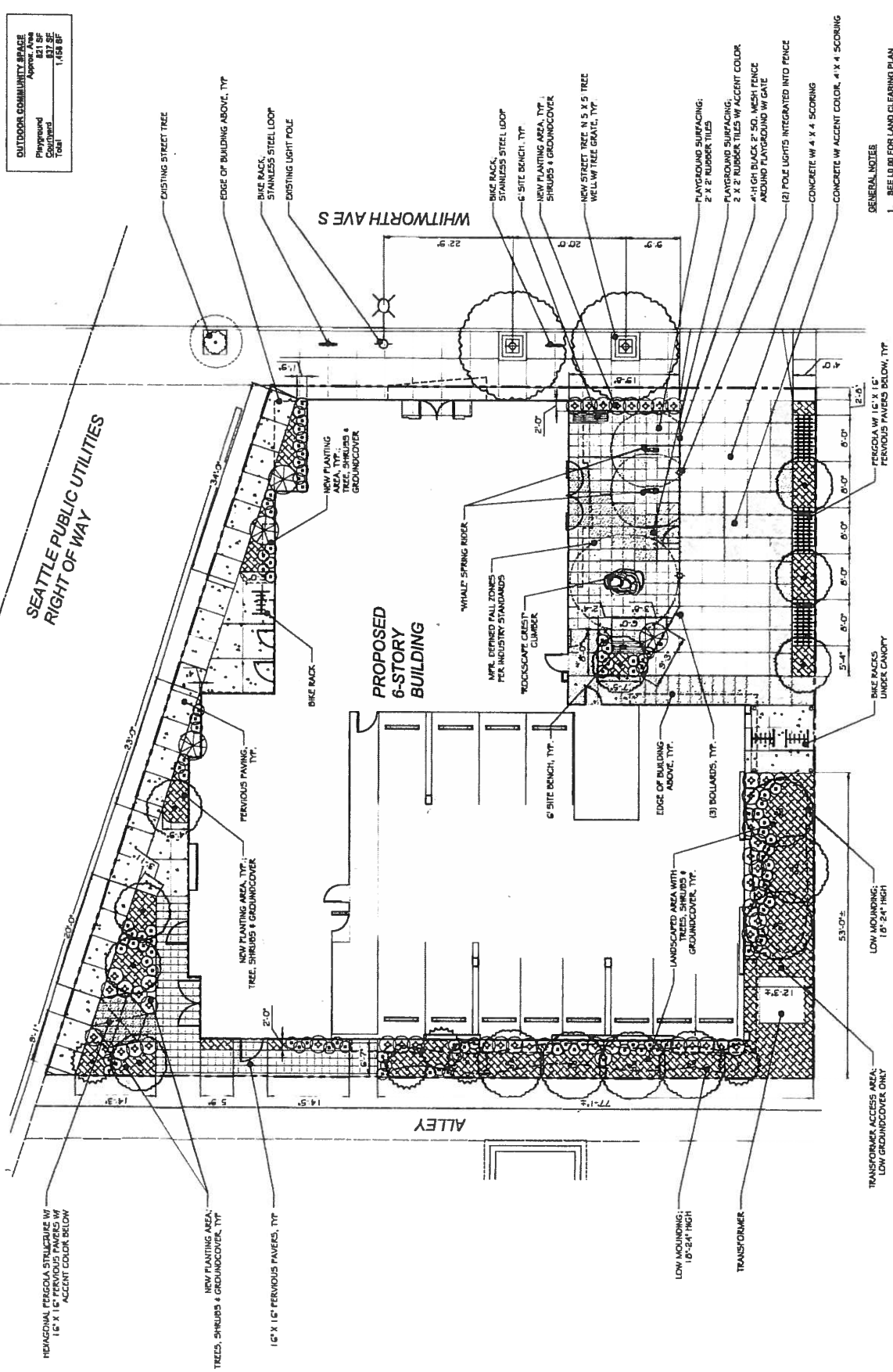
**Project Location Map**

**EXHIBIT 1**



# EXHIBIT 2

[illegible]



- GENERAL NOTES**
- SEE L1.00 FOR LAND CLEARING PLAN.
  - SEE L1.00 SHEET SERIES FOR ADDITIONAL PLANS AND INFORMATION.
  - SEE L2.00 SHEET SERIES FOR PLANTING SCHEDULE, NOTES AND DETAILS.
  - ALL PLANTING MATERIALS SHALL BE PRE-CITY OF RENTON STANDARD PLANS AND SPECIFICATIONS.



1 CONCEPTUAL GROUND LEVEL LANDSCAPE PLAN  
 WPT-18



**EXTERIOR MATERIALS LEGEND**

- A CERAMATIC PANELS - GREEN
- B CERAMATIC PANELS - WHITE
- C CERAMATIC PANELS - CHARCOAL
- D CERAMATIC PANELS WITH 1\"/>

**EXHIBIT 4**

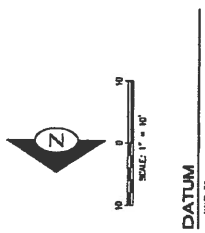


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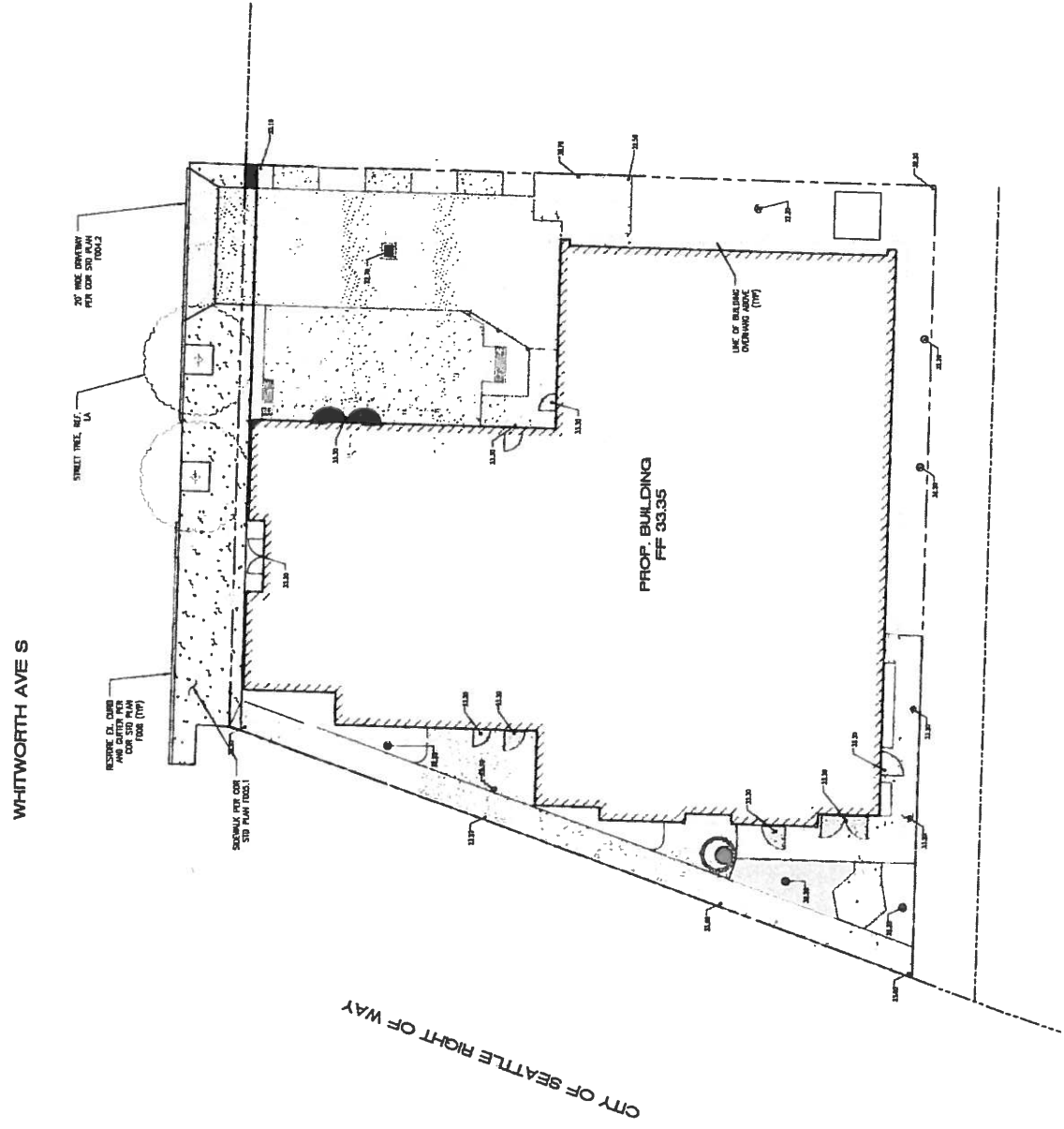
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 NOT FOR CONSTRUCTION

GRADING  
 AND PAVING  
 PLAN



- LEGEND**
- CONCRETE DRIVE
  - PERMEABLE PAVEMENT
  - CONCRETE DRIVEWAY
  - RENDER TILE SURFACING PER LA
  - PROPOSED F.O. ELEVATION

- NOTES**
- SEE SHEET C4.1 FOR GENERAL NOTES
  - ALL SURFACING AND PAVING SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION, AS AMENDED, AND THE CITY OF SEATTLE'S STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION, AS AMENDED.
  - SEE ARCHITECT'S PLANS FOR ADDITIONAL SURFACING INFO AND SPECIAL FINISHES/NOTES.
  - FINISH GRADE INDICATED IS FINAL SURFACE ELEVATION FOLLOWING PLACEMENT OF ALL SURFACING MATERIALS.
  - GRADE ALL AREAS TO PROVIDE DRAINAGE AWAY FROM THE BUILDINGS. FINE GRADE AREAS TO DIRECT DRAINAGE TO DRAINAGE STRUCTURES.
  - COORDINATE GRADING AROUND BUILDING WITH ARCHITECTURAL NOTES. VERIFY LOCATION OF ANY DISCREPANCIES.
  - REVISIONS FOR ALL UTILITY CUTS IN EXISTING PAVEMENT SHALL BE PER CITY OF SEATTLE'S STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION, AS AMENDED, AND THE CITY OF SEATTLE'S STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION, AS AMENDED. AND SHALL PAY ALL COSTS FOR REPAIRS.



July 31, 2015

JN 15274

Low Income Housing Institute for Renton Commons LLC  
2407 First Avenue  
Seattle, Washington 98121

Attention: Robin Amadon

*via email: ramadon@lihi.org*

Subject: **Transmittal Letter – Geotechnical Engineering Study**  
Proposed Apartment Building  
215 Whitworth Avenue South  
Renton, Washington

Dear Ms. Amadon:

We are pleased to present this geotechnical engineering report for the apartment building to be constructed in Renton, Washington. The scope of our services consisted of exploring site surface and subsurface conditions, and then developing this report to provide recommendations for general earthwork and design criteria for foundations and retaining walls. This work was authorized by your acceptance of our proposal, P-9192, dated May 15, 2015.

The attached report contains a discussion of the study and our recommendations. Please contact us if there are any questions regarding this report, or for further assistance during the design and construction phases of this project.

Respectfully submitted,

GEOTECH CONSULTANTS, INC.



Thor Christensen, P.E.  
Senior Engineer

TRC/DRW: at

**CONDITIONAL USE PERMIT**  
**TECHNICAL INFORMATION REPORT**

Project: Renton Commons  
215 Whitworth Ave  
Renton, WA 98057

Prepared For: Tonkin Architecture  
204 1<sup>st</sup> Ave South  
Seattle, WA 98104

Prepared By: Ryan Yokum, EIT  
Reviewed By: Marc Errichetti, PE

Date: June 7, 2016



**EXHIBIT 7**



Gibson Traffic (

Transportation Planners and Traffic Engineers

FULL DOCUMENT AVAILABLE  
UPON REQUEST

## MEMORANDUM

To: Eric Blank, Low Income Housing Institute  
From: Brad Lincoln, PE  
Project: Renton Commons  
GTC #16-088  
Subject: Traffic Analysis  
Date: April 18, 2016



This memorandum summarizes the trip generation calculations for the Renton Commons development. The development is located on the west side of Whitworth Avenue S, between S 2<sup>nd</sup> Street and S 3<sup>rd</sup> Street. The development is proposed to consist of a 6-story, 48-unit apartment building for low-income housing. A site vicinity map is included in Figure 1.

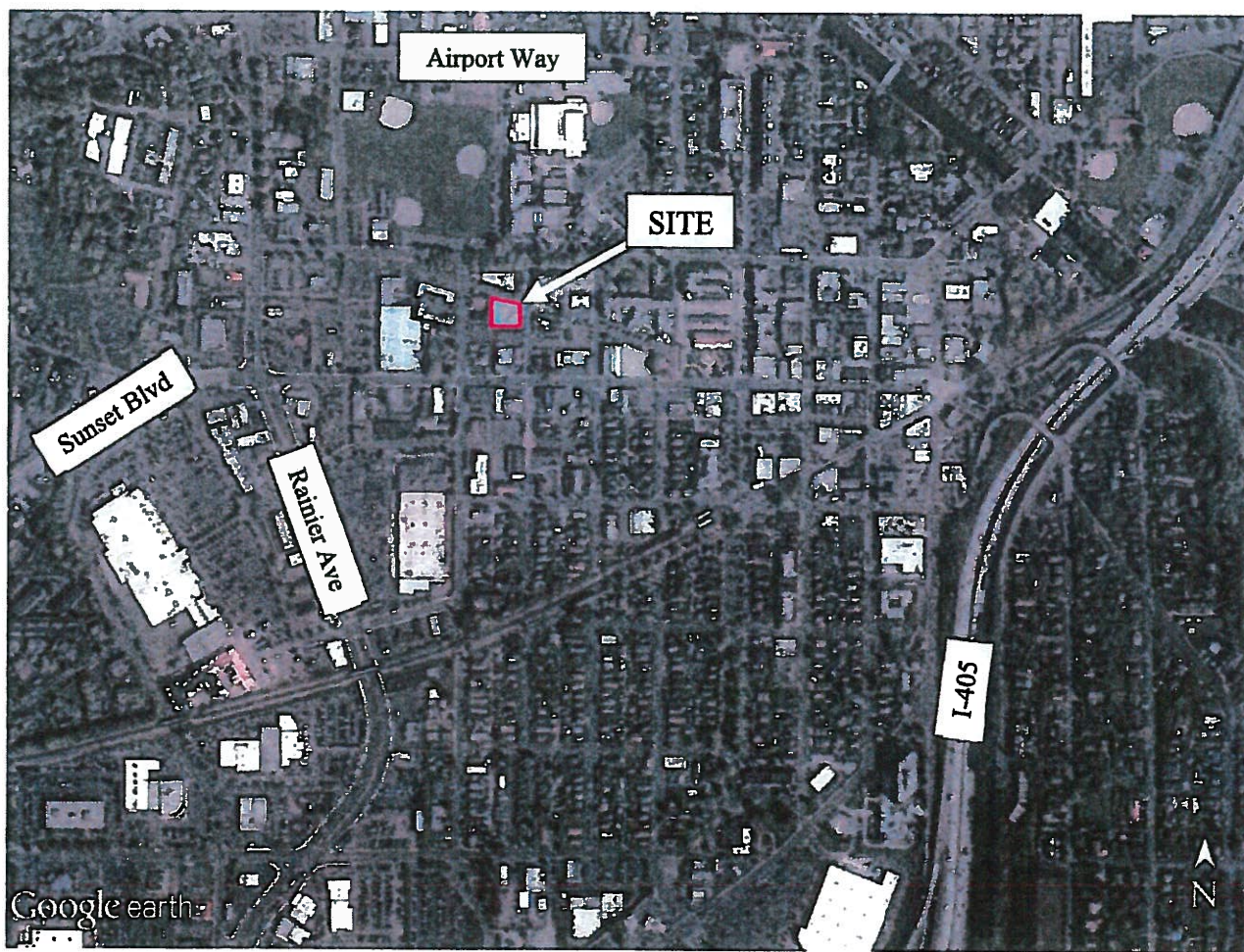


Figure 1: Site Vicinity



July 6, 2016

Ms. Rocale Timmons  
Associate Planner  
City of Renton  
1055 South Grady Way  
Renton, WA98507

In future correspondence please refer to:  
Project Tracking Code: 2016-04-02586  
Property: LUA16-000425, ECF, SA-H, CU-H MOD Renton Commons  
**Re: Archaeology - Survey Requested, More information requested**

Dear Ms. Timmons:

Thank you for contacting the Washington State Historic Preservation Officer (SHPO) and Department of Archaeology and Historic Preservation (DAHP) and providing documentation regarding the above referenced project. As a result of our review, our professional opinion is that the project area has the potential to contain archaeological resources. Identification of cultural resources during construction is not a recommended detection method because inadvertent discoveries often result in costly construction delays and damage to the resource. We request a professional archaeological survey of the project area be conducted prior to ground disturbing activities. The survey should be provided to DAHP and the interested Tribes prior to ground disturbance. We also recommend consultation with the concerned Tribes' cultural committees and staff regarding cultural resource issues.

If any federal funds or permits are associated with this proposal, Section 106 of the National Historic Preservation Act, as amended, and its implementing regulations, 36 CFR 800, must be followed. This is a separate process from both the NEPA and SEPA environmental review processes and requires formal government-to-government consultation with the affected Tribes and the SHPO. Also, we appreciate receiving any correspondence or comments from concerned tribes or other parties concerning cultural resource issues that you receive.



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**INTERIM MEMORANDUM**

**Preliminary Archaeological Assessment for the Proposed Renton Commons Project  
Renton Washington**

**Stephenie Kramer, M.S.**

**May 20, 2016**

**Introduction**

The Low Income Housing Institute (LIHI) proposes construction of the Renton Commons, a multi-family housing development at 215 S. Whitworth Avenue in downtown Renton, Washington. LIHI contracted with Willamette Cultural Resources Associates Ltd. (WillametteCRA), to conduct an archaeological assessment of the proposed project area. WillametteCRA has commenced working on the survey project; this brief memorandum summarizes current field information and progress, provides information regarding the results of archaeological background research and field survey and preliminary conclusions based on those findings, and supports the project through the City of Renton's application intake process. A full report that describes the natural and cultural setting of the project and full discussion of methods, results, conclusions, and recommendations will be forthcoming.

The project site is on approximately 1/3 acre currently occupied by a single family dwelling at 215 Whitworth Avenue S., in the City of Renton, King County, Washington (Figure 1). It is a single parcel between 2<sup>nd</sup> and 3<sup>rd</sup> Streets in downtown Renton (Figure 2), T23N R05E, Section 18. The eastern half of the project parcel is occupied by a single family home (parcel 7841800090), constructed in 1930 according to the King County Assessor's Office. The western half is a residential lawn and ornamental shrubs.

The project as proposed would consist of demolition of the existing structure and construction of a six-story building that includes multiple residential units, common areas, classrooms, parking, and utility line upgrades. Design specifications may include a foundation that will be a structural slab

**DETERMINATION OF NON-SIGNIFICANCE-MITIGATED (DNSM)  
MITIGATION MEASURES AND ADVISORY NOTES**

PROJECT NUMBER: LUA16-000425, ECF, CU-H, SA-H, MOD

APPLICANT: Low Income Housing Institute; 2407 First Ave; Seattle, WA 98121

PROJECT NAME: Renton Commons

PROJECT DESCRIPTION: The Low Income Housing Institute (LIHI) is requesting Hearing Examiner Site Plan Review, Hearing Examiner Conditional Use Permit, Environmental 'SEPA' Review, and three modifications for the construction of a 6-story building containing 48 affordable multi-family residential units. The structure would have an average height of 64 feet. The 0.32 acre site is located within the Center Downtown (CD) zoning classification on the west side of Whitworth Ave S just north of S 3rd St at 215 Whitworth Ave S. Vehicular access to the site would be provided via a single entry point from Whitworth Ave S. A total of 12 parking spaces would be provided within the structure. A refuse and recycle modification, from RMC 4-4-090, is being requested in order to reduce the number of required deposit and collection points as well as the size requirements. A bicycle parking modification, from RMC 4-4-050, is being requested in order to reduce the number of required bicycle parking from 24 to 20 stalls. Finally, the applicant is requesting a street modification, from RMC 4-6-060, in order to maintain the existing 60-foot right-of-way, without dedication, and alter the required street cross section. The site is located within a Seismic Hazard Area. There appears to be no other critical areas located on site.

PROJECT LOCATION: 215 Whitworth Ave S

LEAD AGENCY: The City of Renton  
Department of Community & Economic Development  
Planning Division

**MITIGATION MEASURES:**

1. A survey shall be submitted to the Current Planning Project Manager that conforms to the requirements and standards of the Washington State Office of Archaeology and Historic Preservation and must be conducted under the on-site supervision of a state-approved archaeologist prior to construction permit approval.

**ADVISORY NOTES:**

The following notes are supplemental information provided in conjunction with the administrative land use action. Because these notes are provided as information only, they are not subject to the appeal process for the land use actions.



## CITY CENTER COMMUNITY PLAN


Adopted: June 6, 2011


Prepared by: MAKERS architecture and urban design  
The Transpo Group

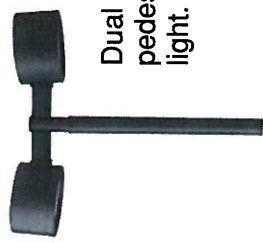
**EXHIBIT 13**


City of  
**Renton**




- 

1 Wall mount LED cylinder downlight
- 

2 3" Cylinder LED downlight
- 

3 Dual Head LED pedestrian scale post light.
- 

4 12"x3" LED forward throw sconce
- 

5 12"x5" shallow lensed cylinder downlight

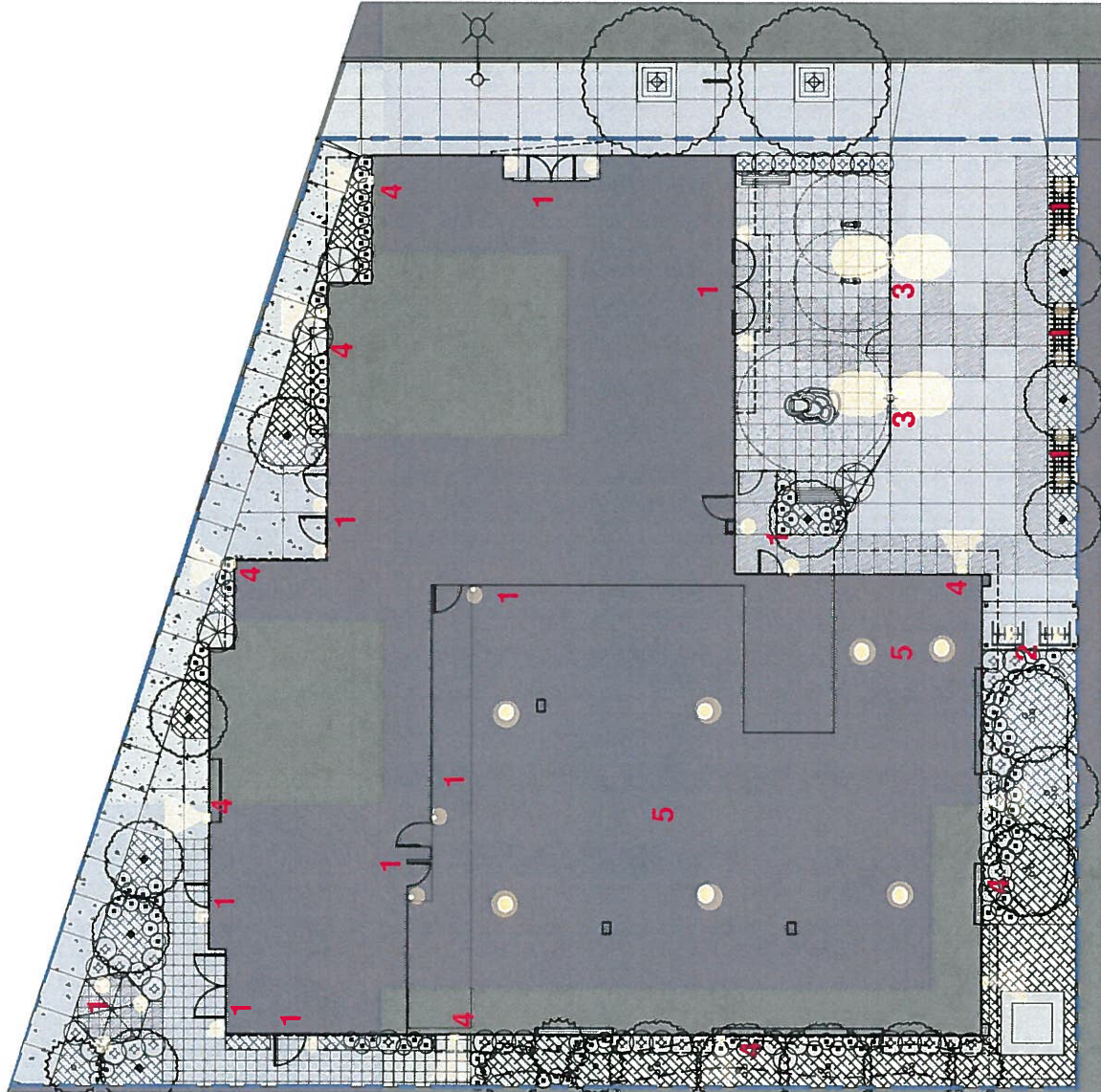


EXHIBIT 14

**RUSHING**  
Engineering & Architecture - Olympia

1725 WESTLAKE AVE N  
SUITE 300  
SEATTLE, WA 98109  
P: (206) 285-7100  
F: (206) 285-7111

**TONKIN**  
architecture

204 First Avenue South  
Seattle, WA 98104  
P: 206-624-7850 F: 206-622-1766  
WWW.TONKINARCHITECTURE.COM

The Renton Commons  
215 Whitworth Ave Renton, WA 98057

Low Income Housing Institute  
2407 1st Ave  
Seattle, WA 98121

05/12/16  
DRAWING ISSUES

PROJECT NO.:  
DRAWING NO.:  
DESIGNED BY:

Lighting  
Glow Plan

**FULL DOCUMENT AVAILABLE  
UPON REQUEST**



# **D O W N T O W N**

## **STREETSCAPE DESIGN STANDARDS AND GUIDELINES**

**FINAL REVIEW DRAFT - MARCH 16, 2016**

**EXHIBIT 15**

EXHIBIT 16



KEY PLAN



WHITWORTH  
AVE S

1 VIEW FROM WHITWORTH AVE - SOUTH  
NOT TO SCALE



2 VIEW FROM WHITWORTH AVE - NORTH  
NOT TO SCALE



AX-1

NOT FOR  
CONSTRUCTION

SCALE	1" = 16'-0"
AUTHOR	ARCHITECT
DATE	JAN 2017
PROJECT	RENTON COMMONS
OWNER	RENTON COMMONS LLC
DESIGNER	TONKIN architecture

MASSING  
MODEL

NO	REV	DATE

RENTON COMMONS LLC  
215 WHITWORTH AVE RENTON, WA 98057

RENTON COMMONS LLC  
215 WHITWORTH AVE RENTON, WA 98057



TONKIN  
architecture

204 First Avenue South  
Seattle, WA 98104  
P 206.624.7800 F 206.622.1766  
www.tonkinarch.com